

**AfDB AND GHA**

Consultancy Services for Detailed  
Engineering Design for Asutuare-Junction-  
Asutuare-Volivo Road (28.3km), Dufor  
Adidome-Asikuma Junction (38.4km) &  
Asutuare-Aveyime Road (24.0km)-Lot-1,  
Ghana

# FULL RESETTLEMENT ACTION PLAN (FRAP)

DECEMBER 2018

MSV INTL USA, KE&T & OWU CONSULT GH

## **Non-Technical Executive Summary**

### **Introduction**

Ghana, located on the West Coast of Africa, is bordered by the Gulf of Guinea in the south, Togo in the East, Cote d'Ivoire in the West, and Burkina Faso in the north. The country has been playing a leading role in West Africa since gaining independence in 1957. Ghana has a population of approximately 25 million (United Nations Development Program, 2011) and a land area of 238,537 km<sup>2</sup>.

The economy of the Country has grown in recent times underpinned by a relatively sound economic management, a growing competitive business environment and improving social and economic infrastructure. The country is endowed with adequate natural resources with agriculture accounting for 25% of Gross Domestic Product (GDP) and employing about 50% of the active work force. Gold, cocoa, timber, diamonds, and bauxite are major sources of foreign exchange. The production of Oil in the Country, which began in December 2010 has also helped sustain economic growth. The nation's Gross National Income (GNI) now stands at US\$ 1,230 per capita (World Bank (WB), 2010). The volume of international cargo meant for its neighboring landlocked countries has been increasing in recent times. There are however challenges which the nation would have to deal with in facilitating transit of vehicles from the neighboring countries, such as chronic congestion in and around Accra, the capital city, and Kumasi, the second largest city.

The situation currently hampers fast and efficient movement of cargo to the neighboring landlocked countries necessitating that an alternative route is identified and developed to facilitate transit and trade. A possible alternative international transit route to Burkina Faso is the Eastern Corridor (N2). The Eastern Corridor which lies to the east of the country, and approximately 695 km in length commences from Tema Roundabout through some important towns in the Greater Accra, Volta, Northern Regions and ends at Kulungugu in the Upper East Region of the country. This corridor is about 200km shorter than the Central Corridor but has a substantial portion unpaved with ageing bridges and experiences washouts and damages during the rainy season making travel difficult.

The Government of Ghana (GoG), in view of the importance of the eastern Corridor, has put a high priority on its development under the Road Sector Medium-Term Development Plan and is actively promoting the development of this route. The development of the Eastern Corridor is expected to contribute to the economic revitalization and the reduction of poverty in the area along the corridor and the neighboring countries by reducing transport cost and facilitating regional trade.

The GoG therefore sought for and obtained support from the Japanese Government for development of a targeted section of the Eastern Corridor. The Japan International Cooperation Agency (JICA), on conclusion of the necessary arrangements between the two Governments, dispatched a team of Experts for the preparatory survey and subsequently a feasibility study of the Asutuare Junction – Volivo Road (28.3km), Dufor Adidome – Asikuma Junction (38.4km) and Asutuare-Aveyime Road (24.0km). It is important to indicate that these road sections were selected in view of the distressed state of the Adomi Bridge and the need to identify roads that could be used as a Bypass around the Bridge. The bridge will be rehabilitated with funds from the Austrian Government but the load that could be applied on the bridge would be reduced significantly when the rehabilitation works are completed. Results of the Feasibility study revealed that the project is feasible.

The Government of Ghana now intends to undertake detailed design of the project roads

and has approached and received a grant from the African Development Fund (ADF) for the financing of the Detailed Engineering Design for Asutware Junction – Volivo Road (28.3km), Dufor Adidome – Asikuma Junction (38.4km) and Asutware-Aveyime Road (24.0km). The Asutware – Aveyime Road is expected to be reclassified as an inter-regional road on completion of the implementation phase of the project.

### **Background of Study Area and Existing Situation**

The Asutware Junction – Volivo Road (28.3km), Dufor Adidome – Asikuma Junction (38.4km) and Asutware - Aveyime Road (24.0km) have immediate impact over 5.4 million inhabitants, majority of whom live below the poverty line. The road traverses an area that is noted for its production of cassava, corn, tomatoes as well as cash and food crops for consumption and exports. The current state of two of the roads if not improved shall result in the total deterioration of the road with the resultant prohibitive capital cost for a total rehabilitation and its social effects. The vehicle operating cost is high. Increased travel times on account of the bad state of the roads become disincentive to business.

- **Asutware Junction – Volivo (28.3km)**

This 28.3km road branches eastwards from Tema-Akosombo road at Asutware Junction and passes through agricultural and commercial towns/villages in the Eastern region to Volivo. The surface is generally gravel but it has some sections paved. The road is of width 7.3m, at the paved section and of lower width at the unpaved sections.

- **Dufor Adidome – Asikuma Junction (38.4km)**

This section currently does not exist and would have to be built through a virgin area. Preliminary road alignment has been developed by the consultant who was engaged for the feasibility study of the road and can be made available.

- **Asutware-Aveyime Road**

This 24km section stretches southwards from Asutware through agricultural and commercial towns/villages in the Volta region to Aveyime. The road is gravel surfaced and is distressed in sections. The road is of width 7.3.

### **Objectives of FRAP**

The main objectives of the FRAP are:

- To prevent or at least mitigate the adverse impacts associated with the implementation of the roads project.
- To ensure that the roads project does not cause major adverse impacts among the people affected.
- To provide guidelines for all aspects of the required resettlement operation in adequate detail that project officers can implement the program successfully, to avoid and reduce impoverishment.
- To deliver the entitlement to the PAPs (Project affected Persons) and support the restoration of their livelihood, in line with 2010 Resettlement Policy Framework (RPF) of the Ministry of Transport (MoT).
- To maximize the involvement of the PAPs in all stages of the implementation of the RAP.

- To ensure that the standard of living of PAPs is improved or at least restored.
- To document the minimization of resettlement through the project design and appropriate methods for adequate compensations, restoration and improved economic livelihoods as well as detailed relocation sites, plans and designs.
- To document specific valuation and compensation strategies that will be adopted for resettling the affected people.
- To assess the capabilities of the agencies responsible, to ensure effective operations including monitoring and evaluation that will provide information on the progress of the program.
- To identify avenues for public information dissemination, public consultation and participation of the people affected which are essential to the success of the project design and implementation.

### **Organizational Responsibilities**

The operational procedures and institutional framework, including principles and role of agencies organizations involved have been discussed for the following institutions: Ghana Highway Authority; Ministry of Transport; Land Valuation Division; Ministry of Finance and Economic Planning; Ministry of Local Government and Rural Development; Lands Commission; Town and Country Planning Department; Environmental Protection Agency; Attorney General Department and Utility Agencies.

### **Community Participation**

Comprehensive engagements and consultations have been held with various relevant stakeholders and the project affected persons from the reconnaissance stage through the public consultations, the project affected persons census along the project roads, a socio-economic survey and an assets inventory. A combined public, community and relevant stakeholder's consultations and engagements were undertaken between 18<sup>th</sup> October and 6<sup>th</sup> December 2014 within the project communities. The issue areas of interest centered on the following: traffic congestion; traffic flow disruption and severance effects; availability of money for the project; deeper consultations with project communities; pedestrian walkway provision; demolition of structures; destruction of paddy rice farms and the fate of rice farmers in the acquisition of new rice farmlands; payment of compensation. After the primary consultations and engagements of project community residents, secondary stage relevant stakeholder consultations and engagements for the proposed roads project were conducted to capture the significant concerns associated with the project from all concerned and interested parties. The process ensured that these groups were given the platform to voice their opinions and views. The process also gave prior disclosure of relevant and adequate project information to stakeholders. The KE&T ESIA/RAP Study Teams held six (6) public meetings on 18/10/2014, 05/12/2014, 06/12/2014, 26/06/2015, 27/06/2015 and 13/11/2015 along the roads project enclave during the development of the ESIA and RAP Reports. Moreover, the KE&T ESIA Study Team, GHA and AfDB Project Design Teams held Stakeholders Community Needs Assessment public meetings on 18/04/2018 at Asutuare Junction, Asutuare, Volivo, Aveyime and Dofor Adidome along the project roads corridor.

### **Socio-Economic Studies**

A census enumeration and socio-economic studies were conducted among the affected project communities. The major objectives of the surveys conducted on the sections 1,3 and 2 of the project roads include the following: (i) to understand the scale and nature of the project impacts on project communities (ii) to identify PAPs and map out their social and economic characteristics (iii) to establish institutional arrangements for the implementation of the RAP activities (iv) to obtain information needed for entitlement and compensation payments for lost assets (v) to generate baseline data for monitoring



and evaluation of livelihoods and income restoration and other sustainable development components. The profile of PAPs captured under the socio-economic studies for the project roads is disclosed as follows:

**Profile of PAPs on Asutuare Jn – Volivo Road (Section 1 Project Road):** Bare Land (186 Males; 105 Females; 2 Unknown and 2 Institution/Public Assets Holders (Total Number of PAPs – 295); Buildings/Structures (73 Males; 39 Females; 9 Unknown and 13 Institution/Public Assets Holders (Total Number of PAPs – 134); Crops/Farms (178 Males; 98 Females; 2 Unknown and None Institutional/Public Assets Holders (Total Number of PAPs – 278). The Final Total Number of PAPs (434 Males; 242 Females; 13 Unknown and 15 Institution/Public Assets Holders) is 707. The socio-economic data was compiled for 679 total number of PAPs for the Section 1 Project Road. The census data was collected from 14/10/2015 to 16/10/2015.

**Profile of PAPs on Dufor Adidome – Asikuma Jn Road (Section 3 Project Road):** Bare Land (83 Males; 47 Females; 2 Unknown and 2 Institution/Public Assets Holders (Total Number of PAPs – 134); Buildings/Structures (41 Males; 12 Females; 4 Unknown and 3 Institution/Public Assets Holders (Total Number of PAPs – 60); Crops/Farms (80 Males; 38 Females; 1 Unknown and 1 Institutional/Public Assets Holders (Total Number of PAPs – 120). The Final Total Number of PAPs (204 Males; 97 Females; 7 Unknown and 6 Institution/Public Assets Holders) is 314. The socio-economic data was compiled for 301 total number of PAPs for the Section 3 Project Road. The census data was collected from 26/10/2015 to 28/10/2015.

**Profile of PAPs on Asutuare – Aveyime Road (Section 2 Project Road):** Bare Land (25 Males; 10 Females; None Unknown and None Institution/Public Assets Holders (Total Number of PAPs – 35); Buildings/Structures (127 Males; 57 Females; 3 Unknown and 17 Institution/Public Assets Holders (Total Number of PAPs – 204); Crops/Farms (116 Males; 93 Females; 5 Unknown and 17 Institutional/Public Assets Holders (Total Number of PAPs – 215). The Final Total Number of PAPs (268 Males; 160 Females; 8 Unknown and 18 Institution/Public Assets Holders) is 454. The socio-economic data was compiled for 428 total number of PAPs for the Section 2 Project Road. The census data was collected from 20/10/2015 to 22/10/2015.

### **Vulnerability Analysis**

Consideration has been given to the needs of disadvantaged groups among those displaced. Vulnerable individuals or groups can be defined as those within a project's area of influence who are particularly marginalized or disadvantaged and who might thus be more likely than others to experience adverse impacts from a project. Vulnerability can be determined by identifying the likelihood that an individual or a group faces harder conditions as the result of the implementation of a project. This vulnerable status may stem from a group's gender, economic status, ethnicity, religion, cultural behaviour, sexual orientation, language or physical and psychological health conditions. Vulnerable groups may include, among others, female-headed households, those below the poverty line, the landless, those without legal title to assets, ethnic, religious and linguistic minorities, Indigenous Peoples, those who are disabled, etc. Vulnerability analysis was undertaken for two (2) categories of vulnerable groups, namely, Vulnerable Female Headed Households and Very Vulnerable PAPs Below Poverty Level (below GHc 100.00 per month or USD 2.00 per day).

**Vulnerable Female Headed Households:** This category was identified along the project roads as follows: Asutuare Jn – Volivo Road (Section 1 Project Road – 13 Females); Dufor Adidome – Asikuma Jn (Section 3 Project Road – 6 Females); Asutuare – Aveyime Road (Section 2 Project Road – 20 Females). The total number of vulnerable female-headed households along the Section 1 Project Road was 39.

**Very Vulnerable PAPs Below Poverty Level (below GHc 100.00 per month or USD 2.00 per day):** This category was identified along the project roads as follows: Asutuare Jn – Volivo Road (Section 1 Project Road – 42 Males and 24 Females); Dufor Adidome – Asikuma Jn Road (Section 3 Project Road – 69 Males and 33 Females) and Asutuare – Aveyime Road (Section 2 Project Road – 28 Males and 17 Females). The total number of very vulnerable PAPs Below Poverty Level was 139 Males and 74 Females i.e. 213 PAPs.

### **Legal Framework, Including Mechanisms for Conflict Resolution and Appeal**

The FRAP has been prepared in accordance with the Ghana Government Legislations and International Applicable Policies and Requirements (including AfDB Guidelines and Safeguard Policies) that govern preparation of Resettlement Action Plans. The relevant national legislations and international applicable policies and requirements considered among others are as follows: (i) Legal Framework for Expropriation and Compensation (ii) Constitution of the Republic of Ghana, 1992 (iii) State Land Act, Act 125 of 1962 (iv) Statutory Wayleave Instrument Act 186 1963 (v) State Lands Act (1962) Act 125 (As Amended) (vi) Ghana Land Policy 1999 (vii) Interest in Land (viii) AfDB Integrated Safeguard on Involuntary Resettlement (2013) (ix) AfDB Gender Policy, 2001 (x) Bank Group Policy on Poverty Reduction, 2004 (xi) Policy and Guidelines on Cooperation with Civil Society Organizations (CSOs), 1999.

### **Comparison of Ghanaian Laws and AfDB Policies and Practices**

Comparison of Ghanaian Laws and AfDB Policies and Practices demonstrate how they address gaps between domestic requirements and international standards and requirement. To overcome these issues, GHA will incorporate a comprehensive gap analysis together with proposed measures to address the gaps in the design of its RAP, thus ensuring full compliance with AfDB OS-2. A matrix table displays both domestic and international requirements and where required states how any gaps identified are addressed.

### **Grievance Redress Mechanism**

In order to deal with the grievances that may arise during the implementation of the FRAP, there is need to incorporate a grievance redress process within the FRAP. During the project implementation, the stakeholders (mostly the communities within the project enclave) could have some grievances with respect to the project activities, their impacts, compensation and other mitigation measures. Some of the key issues of these grievances could include: (i) PAPs not listed (ii) Losses (such as damaged crops) not identified correctly (iii) Compensation inadequate or inappropriate (iv) Dispute about ownership and entitlement (v) Delay in disbursement of compensation (vi) Improper distribution of compensation in case of joint ownership (vii) Misidentification of owner/occupier of eligible property and assets (viii) Errors in asset assessment (ix) Disputes over plots limits, either between the affected person and the project, or between two (2) neighbors' (x) Complaints about entitlement policy of the project (xi) Disagreement of asset valuation (xii) Discontent over time and manner of compensation (xiii) Disputed ownership of businesses (for example if the owner and the operator are different persons) (xiv) Any other issues arising during the project implementation. How a PAP could launch a complaint follows a 6 Tier-System for grievance redress mechanism levels:

**1<sup>st</sup> Tier- System:** A PAP should first raise a grievance process at this level or local (sub-activity level). If the grievance cannot be resolved at this level, it is referred to the next level – **2<sup>nd</sup> Tier-System**. This second level is also known as the GHIDRS (GHA Internal Dispute Resolution System). If the resolution of the grievance proves unsuccessful, it is referred to the **3<sup>rd</sup> Tier-System** or the Project-Level Grievance Resolution or Grievance Redress Committee (GRC). At this third level, if the grievance resolution process fails or proves unsuccessful the complainant needs to move to the **4<sup>th</sup> Tier-System** by procuring the

services of a valuer and submit a report to Land Valuation Division (LVD) of Land Commission. If the PAP is still not satisfied, he/she should ultimately move to the law courts which is the **5<sup>th</sup> Tier-System** or the national legal level. Finally, a complainant who is still dissatisfied under the Laws of the Republic of Ghana should seek redress at the **6<sup>th</sup> Tier-System** or the AfDB Independent Review Mechanism (IRM) for AfDB funded projects (**at the Country Office Only**).

Where to launch a complaint by PAP, currently, is by contacting any of the persons handling grievance redress between now and financial close (until the committees are finally put in place). The details of the persons are: **Mr. Richard Kudjawu (0244659125) – GHA, N2 Project Coordinator; Ms. Hilda Annan (0244991468) – GHA, N2 Project RAP Manager; Mr. Kwaku Anim Boateng (0244657627) – KE&T Consult, N2 Project RAP Consultant.**

## Entitlement Matrix

An entitlement matrix captures all affected parties, the characteristics of the impact, and the types of compensation/reinstatement due them. Categories in the matrix table developed for the entitlement overlap because those being offered re-instatement fall into several groupings and the groupings tend to overlap. For instance, some of the owners of permanent and temporary structures own land as well. Again, some of the business operators also own property such as land or structures. Basically, the entitlement matrix identifies the following losses of assets affected by the project roads RoW alignment or acquisition.

**Loss of Agricultural, Residential, Commercial or Institutional Land** (cover legal title holders, customary usufruct rights holders, compensation of all affected lands will be paid based on RoW acquisition and displacement); **Loss of Residential, Commercial (Business), Institutional Structure** (cover owners of affected structures with or without legal title, will be entitled to compensation at the replacement value without deduction for salvage value of the structures); **Loss of Incomes** (cover business owners/operators, house owners/tenants, land lords/leaser who experience loss of incomes through loss of business income, loss of business rent, loss of residential rent and loss of advance rent for renters will be paid supplementary assistance); **Loss of Standing Crops** (crops takings by the project implementation program will affect cultivators who will be entitled to compensation for damaged crops, based on current market prices); **Loss of Trees** (losses resulting from economic and non-economic trees during vegetation clearing will affect tree owners. Owners of affected trees, with or without land title, will be paid compensation on current market value of the trees, based on valuation results); **Loss of Common Resources and Facilities** (loss of common resources or facilities will be replaced or restored in consultation with the affected project community); **Loss of/or Damage to Cultural Property** (compensation will be made for loss of cultural properties such as shrines, groves, cemetery, mausoleum at current replacement cost or value, based on consultations with the affected community); **Loss of Public Infrastructure** (loss or relocation of affected community infrastructure will attract compensation, based on current value, upon consultations with the affected community);

## Eligibility

Eligibility has been based on the category of losses to be suffered as a result of the roads project implementation process. The category of losses to be suffered will be derived from various interests and rights identified through common law and international conventions. Eligible affected persons have been presented in an Entitlement Matrix attached to this report. Moreover, eligibility has been based on the category of losses suffered as at the **cut-off date was August 15<sup>th</sup>, 2015**. Within the purview of the requirements of Ghanaian law and the Bank, these people are those who: (i) have legal rights to land,

including customary and traditional rights (ii) do not have legal rights to land at the time of the census but have recognized rights of enjoyment or certain claims to the land or property; and (iii) occupy land although they have no legal right to it.

### **Valuation of and Compensation for Losses**

Inspection and referencing of all bare land, buildings/structures and crops/farms falling within RoW was carried out initially on 22/11/2014. However, additional inspections and referencing of all affected assets within RoW due to new changes in alignments made by GHA were captured later. The purpose of the inspection and referencing of the affected properties was to collect the descriptive and other technical data on the affected properties. Both the internal and external areas were thoroughly inspected, except the areas which are not accessible. The existing buildings/structures were profiled and mapped out and measurements taken of all buildings and other structures. Additionally, profiles and mapping of bare lands and crops and farms including their measurements were undertaken. The valuation methodology was applied as follows:

**Valuation of Residential/Commercial/Buildings/Structures** (full replacement cost method was adopted for owners of permanent structures in line with Ghanaian and AfDB requirements. The capital value of an existing development was equated to the cost of reinstating the development on the same plot at current labor, material and other incidental costs. The current cost of construction per square meter was considered based on architectural design, building height, layout, floor spaces, plot size, fittings and fixtures and other physical attributes with cost implications. In assessing replacement cost of civil work improvements, the replacement cost method was applied using analysis of current construction cost of similar works).

**Valuation of Agricultural/Residential/Commercial/Institutional/Land** (value of land was determined by comparing market evidence on recent sale of plots in the vicinity. The rate per acre/hectare was established based on assessing the land value attributes and advantages of the property location. Basically, the affected land areas are the measured land area occupied by the affected building, civil works, farms. Business and institution. The study did not assess the leaseholders' term subsisting in their affected land. A long-term lease hold period of 50 years was adopted. The reversionary interest in these affected lands were not assessed. The compensation estimate covers leaseholders but not freeholder or allodial interest holder namely chiefs and their respective families).

**Valuation of Crops/Farms** (determination of crops compensation rates was made from LVD approved compensation rates and crop rate negotiation committee plus disturbance allowance. Annual and seasonal crops were not compensated since PAPs will harvest before the project implementation; Valuation of Loss of Business/Income (loss of profit allowance was determined from assessing the net profit per month multiplied by the duration of construction. The possible loss of the business' Net Profit during the relocation of structures and commencement of business at new sites were assessed and applied. Significant factors considered were the business nature, location attributes and commercial characteristics of the project road corridor. Net income was assessed based on a reinstatement period of 6-months).

**Valuation of Common Resources and Facilities/Cultural Properties/Public Infrastructure** (general estimates from the construction industry will be applied to calculate the expected value of affected assets. Moreover, replacement values of affected assets will be sought).

**Supplemental Assistance to Vulnerable People and Owners of Temporary Structures** (PAPs under this category will receive additional amount, equivalent to 15% and 20% of the value of the compensation to be provided respectively for vulnerable and very vulnerable persons. The supplemental assistance in the form of cash will cover the following: (i) cost of removal and transporting structures relocation cost (ii) loss of business during the transition period of relocating (iii) loss of business goodwill (iv) loss of connection to utilities (electricity) (v) loss of mass concrete floors, paved frontage and other civil work improvement).

**Disturbance** (covers expense and other losses in assessment of land, buildings and business relocation. Affected PAPs operating businesses will be paid additional sum of money for loss of business goodwill, reconnection to electricity, moving of stock/goods and other intangible losses due to acquisition and non-enjoyment of the property. Disturbance was assessed as a percentage of cost and ranges from 2% to 10% of the total supplementary assistance.

**Containers/Kiosks** (payment of supplemental assistance to owners of temporary structures will also be made. The assessment of supplemental assistance was based on the premise that temporary structures could be moved and relocated. Anticipated cost of transporting/moving the structures to the designated location, including cost of concrete works installation will be made.

#### **Identification and Selection of Resettlement Site, Site Preparation and Relocation**

All PAPs will not have to be relocated on a specific relocation site. Therefore, there are no measures to be taken to address this concern. Invariably, the issue with identification and selection of resettlement site, including site preparation and relocation do not apply to this Proposal. Data from the socio-economic survey indicated the following preferred mode of compensation characteristics along the project roads:

**Preferred Mode of Compensation on Section 1 Project Road (Asutware Jn – Volivo Road):** Total Number of PAPs (679) – 79% (cash compensation); 9% (Government should decide); 4% (land-for-land); 3% (resettlement and cash compensation); 2% (resettlement at new location and undecided) and 1% (replace damaged structures sections).

**Preferred Mode of Compensation on Section 3 Project Road (Dufor Adidome – Asikuma Jn Road):** Total Number of PAPs (301) – 80.0664% (cash compensation); 13.2890% (Government should decide); 1.9934% (land-for-land); 1.6611% (undecided); 1.3289% (resettlement at new location); 0.9967% (resettlement and cash compensation) and 0.6645% (replace damaged structures sections).

**Preferred Mode of Compensation on Section 2 Project Road (Asutware – Aveyime Road):** Total Number of PAPs (428) – 79.9065% (cash compensation); 9.3458% (Government should decide); 4.6729% (resettlement and cash compensation); 3.5047% (undecided); 1.4019% (land-for-land); 0.7009% (resettlement at new location) and 0.4673% (replace damaged structures sections).

#### **Environmental Protection**

Environmental protection will be ensured in accordance with Environmental and Social Management Plan (ESMP) prepared as a stand-alone document. Moreover, environmental protection measures will be implemented as contained in the Environmental and Social Impact Assessment (ESIA) report.

## Implementation Schedule

All assets acquisition for the roads project will be completed before the start of construction work at sections where resettlement issues are envisaged. And all such land sections will be provided free of encumbrances. All affected assets owners will be given adequate notice to vacate those drainage sections with affected assets. The implementation schedule for the roads project is given in a matrix format table. It should be noted that implementation periods shall change depending on when the financial resources shall be finalized. However, the change in dates shall not affect the actual periods. Each step shall not necessarily change. It is important that all RAP activities including payments for compensation are completed before roads project construction begins. This is to ensure that all possible hindrances to the project implementation will have been dealt with. However, some grievances may arise during the project implementation.

## Costs and Budget

The overall estimated provisional cost for implementing the RAP is **GHc 26,292,463.89** of which **GHc 4,563,154.89** is earmarked for RAP implementation and monitoring. The bulk of this cost is the full replacement costs of immovable permanent structures within the road reservation for which some demolition is triggered. The RAP costs are expected to be met by Ghana Government through Ministry of Finance and hence be budgeted for accordingly. However, GHA will be the Implementing Agency for the RAP.

## Monitoring and Evaluation

Project monitoring and evaluation will include internal and external monitoring, AfDB supervision missions, a mid-term review and a final evaluation incorporating the completion report of the implementing agencies. GHA will supervise the Implementation Program and ensure the timely execution of roads project activities. To enable GHA effectively undertake the monitoring, a Resettlement Monitoring Committee will be set up at the beginning of the implementation period to coordinate the various aspects of the resettlement / displacement process. RMC meetings will be held every month for the initial resettlement period of say eight (8) months, thereafter quarterly for the rest of the project implementation period. GHA will co-chair the committee with the Civil Society Representative. This project is adopting a participatory monitoring and evaluation strategy where all stakeholders will be involved to monitor the various stages of resettlement and jointly evaluate the FRAP after all the processes have been completed. Specific indicators shall be monitored to ensure that notices are served in a timely manner to allow property owners to vacate the RoW immediately after community sensitization and public awareness. Monitoring plans will be developed to cater for all three (3) phases of the project implementation program: (i) monitoring during pre-construction (ii) monitoring during construction (iii) monitoring during operation.

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## 1 Introduction

Ghana, located on the West Coast of Africa, is bordered by the Gulf of Guinea in the south, Togo in the East, Cote d'Ivoire in the West, and Burkina Faso in the north. The country has been playing a leading role in West Africa since gaining independence in 1957. Ghana has a population of approximately 25 million (United Nations Development Program, 2011) and a land area of 238,537 km<sup>2</sup>.

The economy of the Country has grown in recent times underpinned by a relatively sound economic management, a growing competitive business environment and improving social and economic infrastructure. The country is endowed with natural resources with agriculture accounting for 25% of Gross Domestic Product (GDP) and employing about 50% of the active work force. Gold, cocoa, timber, diamonds, and bauxite are major sources of foreign exchange. The production of Oil in the Country, which began in December 2010 has also helped sustain economic growth. The nation's Gross National Income (GNI) now stands at US\$ 1,230 per capita (World Bank (WB), 2010). The volume of international cargo meant for its neighboring landlocked countries has been increasing in recent times. There are however challenges which the nation would have to deal with in facilitating transit of vehicles from the neighboring countries, such as chronic congestion in and around Accra, the capital city, and Kumasi, the second largest city.

The situation currently hampers fast and efficient movement of cargo to the neighboring landlocked countries necessitating that an alternative route is identified and developed to facilitate transit and trade. A possible alternative international transit route to Burkina Faso is the Eastern Corridor (N2). The Eastern Corridor which lies to the east of the country, and approximately 695 km in length commences from Tema Roundabout through some important towns in the Greater Accra, Volta, Northern Regions and ends at Kulungugu in the Upper East Region of the country. This corridor is about 200km shorter than the Central Corridor but has a substantial portion unpaved with ageing bridges and experiences washouts and damages during the rainy season making travel difficult.

The Government of Ghana (GoG), in view of the importance of the eastern Corridor, has put a high priority on its development under the Road Sector Medium-Term Development Plan and is actively promoting the development of this route. The development of the Eastern Corridor is expected to contribute to the economic revitalization and the reduction of poverty in the area along the corridor and the neighboring countries by reducing transport cost and facilitating regional trade. The GoG therefore sought for and obtained support from the Japanese Government for development of a targeted section of the Eastern Corridor. The Japan International Cooperation Agency (JICA), on conclusion of the necessary arrangements between the two Governments, dispatched a team of Experts for the preparatory survey and subsequently a feasibility study of the Asutwae Junction – Volivo Road (28.3km), Dufor Adidome – Asikuma Junction (38.4km) and Asutwae - Aveyime Road (24.0km). It is important to indicate that these road sections were selected in view of the distressed state of the Adomi Bridge and the need to identify roads that could be used as a Bypass around the Bridge. The bridge will be rehabilitated with funds from the Austrian Government but the load that could be applied on the bridge would be reduced significantly when the rehabilitation works are completed. Results of the Feasibility study revealed that the project is feasible.

The Government of Ghana now intends to undertake detailed design of the project roads and has approached and received a grant from the African Development Fund (ADF) for the financing of the Detailed Engineering Design for Asutwae Junction – Volivo Road (28.3km), Dufor Adidome – Asikuma

Junction (38.4km) and Asutware - Aveyime Road (24.0km). The Asutware – Aveyime Road is expected to be reclassified as an inter-regional road on completion of the implementation phase of the project.

### 1.1 Objectives of the Full Resettlement Action Plan (FRAP)

The main objectives of the FRAP are:

- To prevent or at least mitigate the adverse impacts associated with the implementation of the roads project.
- To ensure that the roads project does not cause major adverse impacts among the people affected.
- To provide guidelines for all aspects of the required resettlement operation in adequate detail that project officers can implement the program successfully, to avoid and reduce impoverishment.
- To deliver the entitlement to the PAPs (Project affected Persons) and support the restoration of their livelihood, in line with 2010 Resettlement Policy Framework (RPF) of the Ministry of Transport (MoT) and applicable AfDB's applicable requirement's such as OS-1, OS-2, OS-4 and OS-5.
- To maximize the involvement of the PAPs in all stages of the implementation of the FRAP.
- To ensure that the standard of living of PAPs is improved or at least restored.
- To document the minimization of resettlement through the project design and appropriate methods for adequate compensations, restoration and improved economic livelihoods as well as detailed relocation sites, plans and designs.
- To document specific valuation and compensation strategies that will be adopted for resettling the affected people.
- To assess the capabilities of the agencies responsible, to ensure effective operations including monitoring and evaluation that will provide information on the progress of the program.
- To identify avenues for public information dissemination, public consultation and participation of the people affected which are essential to the success of the project design and implementation.

### 1.2 Scope of the FRAP

The scope of the FRAP is detailed as follows:

**FRAP Population Census and Socio-Economic Survey:** This identified all affected persons and the extent to which they have been affected. These also included households, businesses and lands.

**PAP Asset Inventory and Valuation:** Houses, businesses and landed properties, including, other infrastructural assets were not assessed and valued as losses. Partial loss of property was also not taken into consideration.

**Resettlement Area Assessment:** This involves the identification of areas to relocate affected individual house owners, business owners and various categories of hawkers (where applicable) for possible relocation or supplemental remedial measures. But resettlement area assessment was not undertaken as part of the scope of the FRAP due to financial resources constraint.

### 1.3 Methodology by Tasks

**Field Inspections and Trekking:** Critical observations were made to confirm and establish the existence of resettlement/dislocation issues of concern within the project roads zone of influence. Such issues and



conditions are very likely to develop from the proposed roads project implementation. Through field inspection and trekking, data was collected and compiled to identify significant triggers and drivers of resettlement, dislocation and displacement effects, leading to development of a full-blown RAP development.

**RAP Asset Identification and Inventory:** In establishing the extent of PAP's losses, an asset inventory and identification were records made. The inventory detailed which assets would be partly and/or completely be demolished or would need to be relocated. Ownership information was established during the survey.

**Project Communities Physical Infrastructure Identification and Inventory:** Critical attention was given to project communities' common physical assets or resources affectation by the roads project. Observational studies helped to establish the types of project community assets likely to be affected by the roads project, including cemeteries, public water stand pipes, boreholes and playing fields.

**Resettlement Areas for Common Resources:** Potential relocation sites as possible alternative sites to accommodate displaced common resources or facilities could not be reviewed within the roads project corridor, due to lack of potentially open and available spaces. However, potentially feasible sites which appear to be most suitable for the required purposes would be subjected to further scrutiny for final approval and selection.

**Stakeholders Participatory Program Assessment:** Extensive and deep consultations have been carried out with all relevant stakeholders to bring out peoples' views and perceptions about the project. Lessons have been learnt on how they would like to be involved in the consultation process. A public forum would be held during the RAP implementation to disseminate information and solicit more views.

**Institutional Identification and Capacity Assessment of Agencies:** Institutions that have role to play in the resettlement program have been categorized and their capacities assessed through a study of earlier works and a rapid appraisal done by the RAP team.

**Census and Socio-Economic Survey:** A listing of all persons who own land, structures, crops, businesses, tenants of shops, homes, etc., have been prepared through the administration of questionnaires. In terms of roads project impact, these are people likely to lose shelter, business premises and other assets and suffer other intangible losses. Data gathering included photographs of PAPs, bio-data, educational levels, nationality, ethnicity, religion and status as well as activity in relation to the roads project program. The ESIA documentation has supplemented this numeration assessment where applicable.

**Primary Data Collection:** Primary data collection involved the combination of observational studies, use of trometer and measuring tape to measure the right-of-way width of the project roads. This helped to establish extent of PAPs assets displacement, in-depth field inventory taking and assessment and interviews with individual roads project residents (PAPs and non-PAPs).

**Data Collation and Analysis:** The quantitative data from the field inventory have been subjected to both manual and computerized programming for analysis. Assets of PAPs have been photographed and the respective chainages established to make location of PAPs easier later. Connecting PAPs to specific assets have been noted.

**Review of Project Document:** Considerable numbers of previous publications on the project or similar ones have been reviewed. These documents have helped to set the roads project within the relevant background.

**Sectional Approach for RAP Implementation:** Usually, limited time is involved in RAP implementation and construction schedules. Therefore, the RAP Consultant worked closely with the engineering team to identify project roads sections for RAP and construction implementation. As a result, the roads project program will be divided into specific implementation sections according to the chainage and construction priorities based on required construction period.

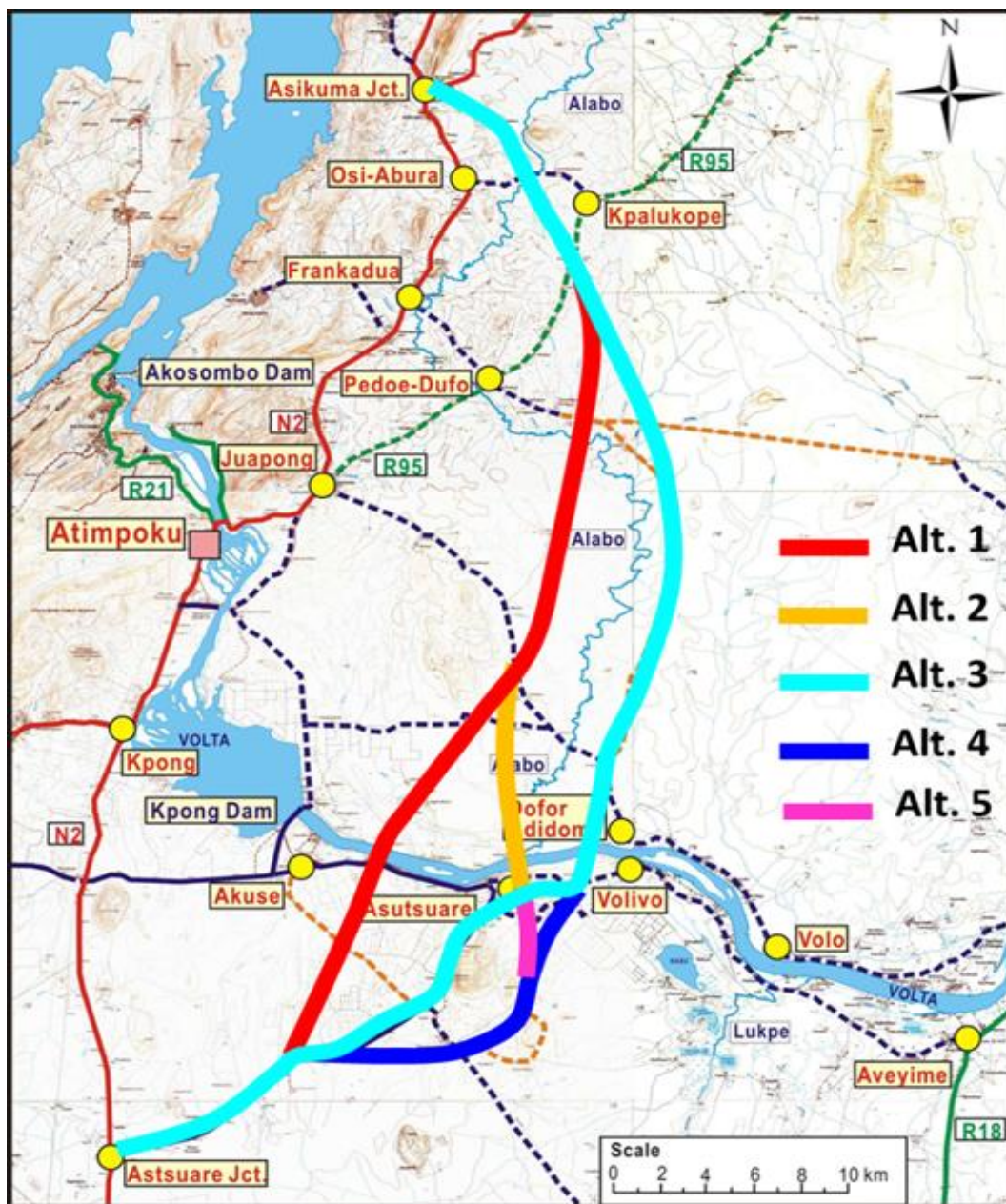
**RAP Implementation Practices:** These approaches will be adopted to facilitate the implementation of the RAP:

- Assignment of identification numbers to each PAP to help identify themselves in relation to their claim. This number will be presented during payment of their claim.
- Use of electronic payment system instead of manual payment to minimize the risk associated with handling of cash.
- Establishment of GHA offices within the roads project corridor to facilitate payments to PAPs seeking their compensation packages and will establish electronic payment at those locations.
- PAPs should present proof of ownership of affected assets before negotiation commences.

#### 1.4 Project Background

The Proposal has the following background information: (i) the Eastern Corridor (N2) is an alternative international transit route identified by the Government of Ghana (GOG) to facilitate transit and trade; (ii) the GOG sought for and obtained support from the Japanese Government to develop the N2; (iii) JICA undertook a feasibility study and established that the implementation of N2 is feasible; (iv) GOG now intends to develop the N2 based on JICA's feasibility studies and designs; (v) JICA's feasibility studies and design yielded five alternative road alignments (see map); (vi) JICA's preliminary road alignment (alternative 3) was selected by the Client for further studies and development (see map).

This Full Resettlement Action Plan (FRAP) will cover the following topics: description of the project; project area and area of influence; potential impacts; organizational responsibility and institutional framework; community participation; integration with host communities; policy and legal framework; grievance handling procedure; eligibility; valuation of and compensation for losses; implementation timetable of the FRAP; cost and budget; monitoring and evaluation.



**Figure 1: Alternative Routes Provided by JICA**

### 1.5 Project Location

The Eastern Corridor which lies to the east of the country, and approximately 695 km in length commences from Tema Roundabout through some important towns in the Greater Accra, Volta, Northern Regions and ends at Kulungugu in the Upper East Region of the country. This corridor is about 200km shorter than the Central Corridor but has a substantial portion unpaved with ageing bridges and experiences washouts and damages during the rainy season making travel difficult. The GoG intends to develop a targeted section of the Eastern Corridor which traverses eight (8) districts and one (1) metropolis in three (3) regions. Therefore, the project zone will cover the following districts: Yilo Krobo, Manya Krobo, Asuogyaman (in the Eastern Region); Dangme West and Dangme East (in the Greater Accra Region); South Tongu, North Tongu, Adaku-Anyigbe (in the Volta Region) and Ho metropolis (in the Volta Region).

The Proposed Project Roads are the following:

- **Section 1: Asutuare Junction – Volivo (28.3km)**

This 28.3km road branches eastwards from Tema-Akosombo road at Asutuare Junction and passes through agricultural and commercial towns/villages in the Eastern region to Volivo. The surface is generally gravel but it has some sections paved. The road is of width 7.3m, at the paved section and of lower width at the unpaved sections.

- **Section 3: Dufor Adidome – Asikuma Junction (38.4km)**

This section currently does not exist and would have to be built through a virgin area. Preliminary road alignment has been developed by the consultant who was engaged for the feasibility study of the road and can be made available.

- **Section 2: Asutuare-Aveyime Road (24km)**

This 24km section stretches southwards from Asutuare through agricultural and commercial towns/villages in the Volta region to Aveyime. The road is gravel surfaced and is distressed in sections. The road is of width 7.3m.



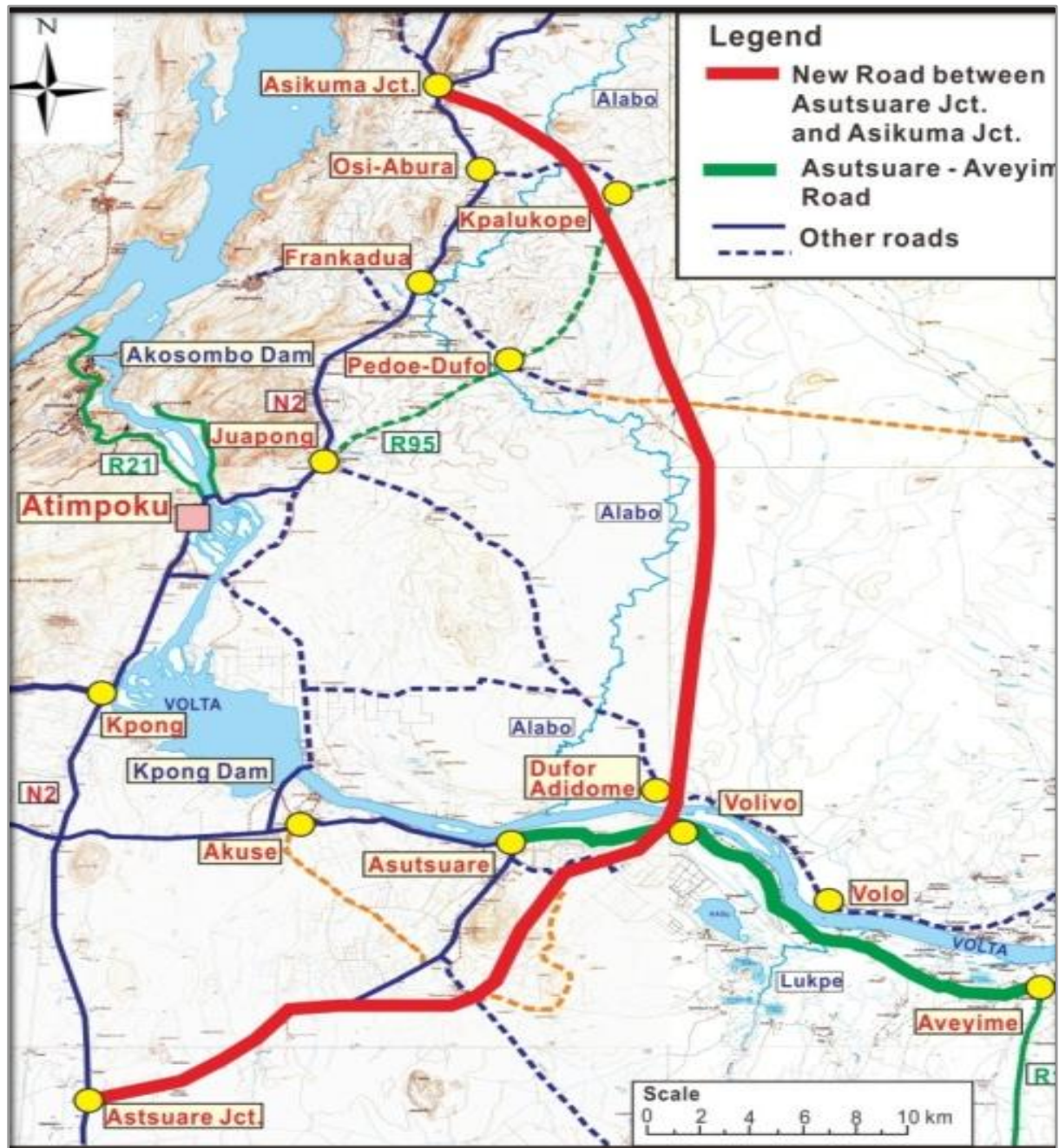


Figure 2: Map Showing the Selected Alignment

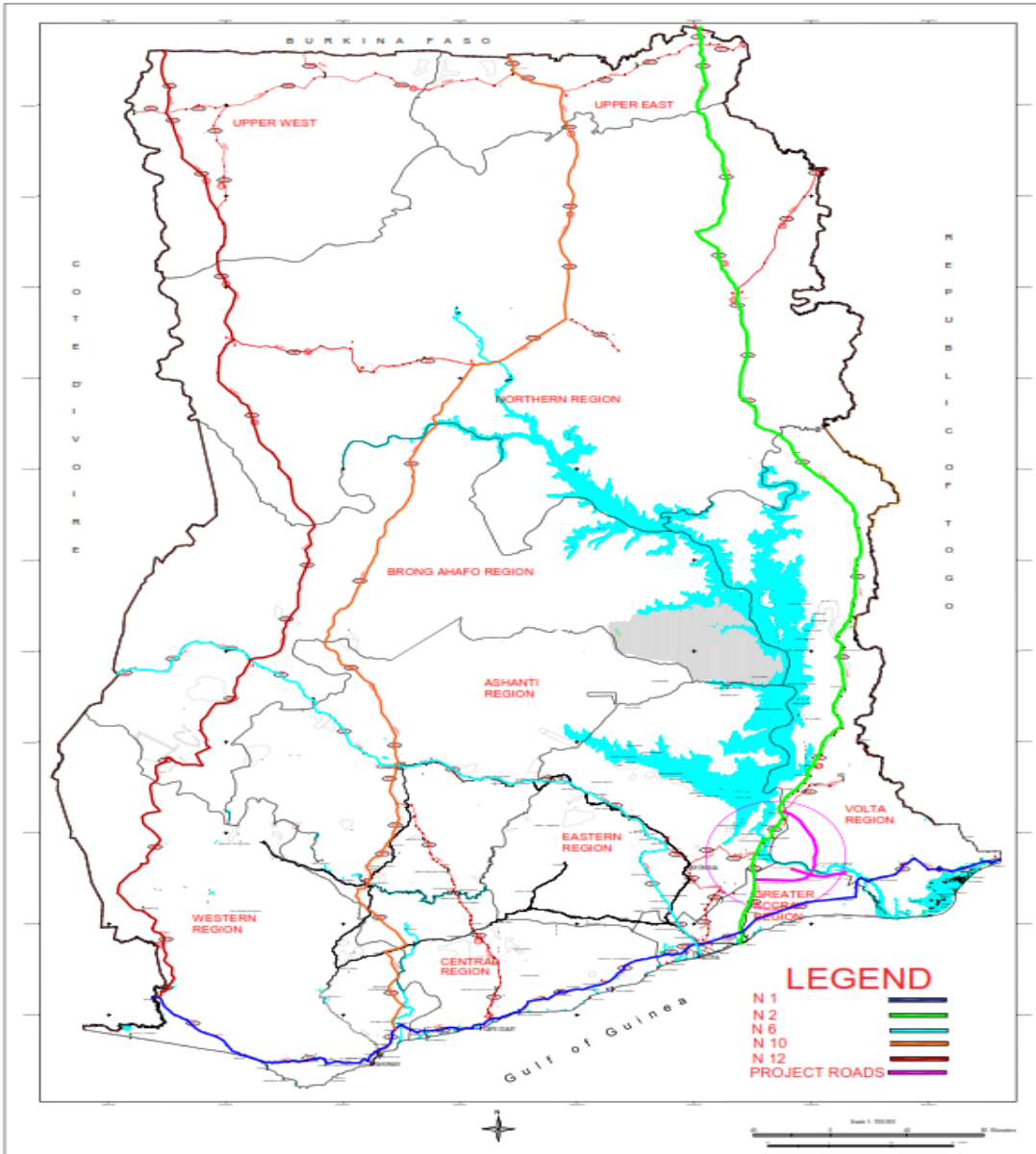


Figure 3: Ghana Map Showing Connectivity between Project Roads and the National Road Network

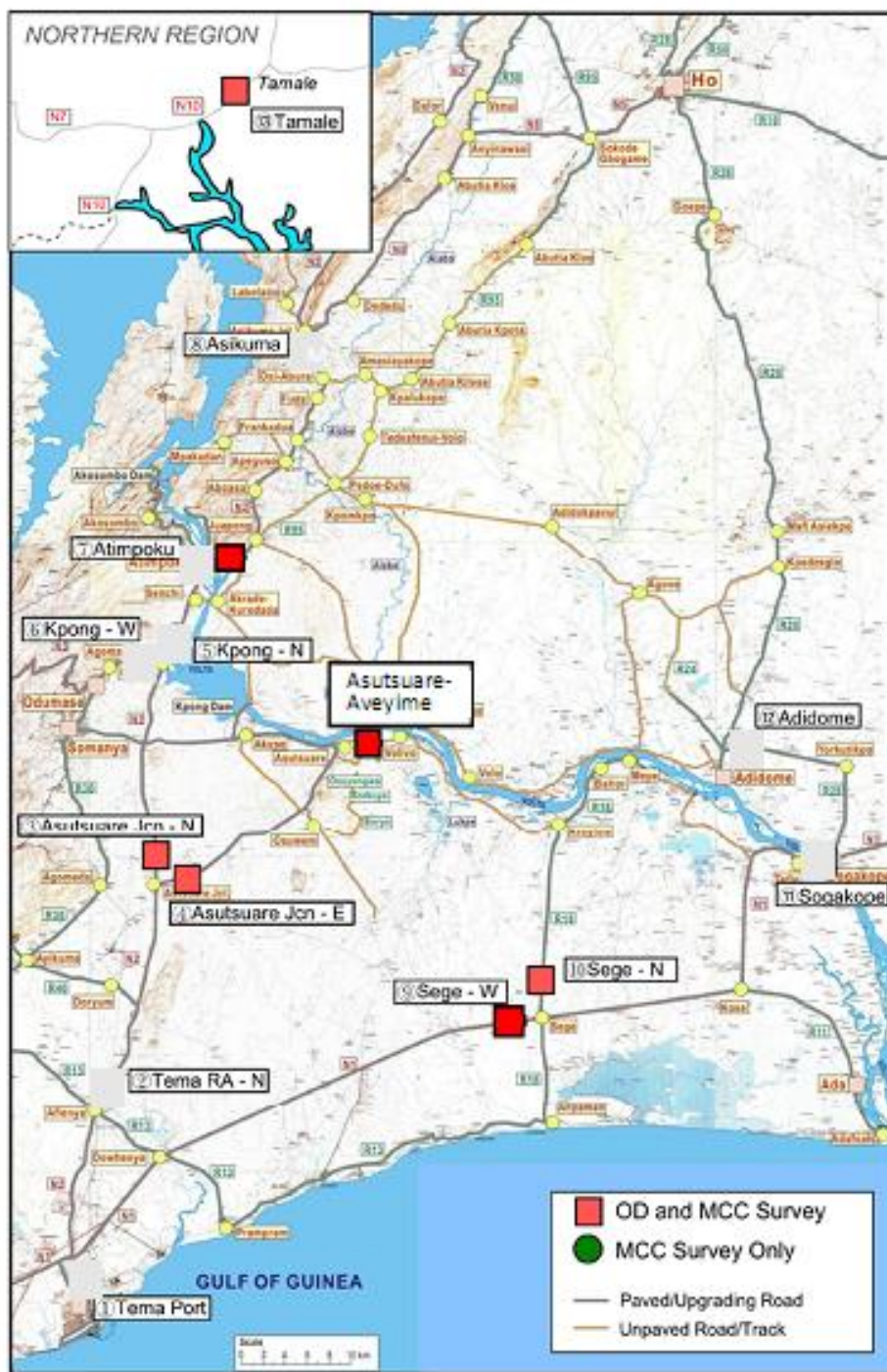


Figure 4: Classified Manual Counts (MC) and Origin-Destination (OD) Surveys Census Locations

## 1.6 Project Description

### 1.6.1 Field Study Findings of Proposed Alignments (Sections 1 and 3)

- **Section 1: Asutuare Jn – Volivo Road (28.3km)**
- **Section 3: Dufor Adidome – Asikuma Jn (38.4km)**

#### 1.6.1.1 Hydrological and Drainage Conditions of Sections 1 and 3

There are 2No. Pipe Culverts and 9No. Box Culverts as definite drainage arrangements within the entire Section 1 of the proposed road. On the other hand, there are no pipe culverts nor box culverts adorning Section 3 of the proposed project road. Instead, River Volta, River Alabo and tributaries of River Lomen cross Section 3 of the proposed project road. The existing drainage structures within Section 1 are structurally weak with inadequate hydraulic capacities, therefore, would have to be demolished and replaced. **See Table 1.0.**

**Table 1.0 Hydrological/Drainage Findings for Sections 1 and 3 of Project Roads**

Serial Number	Number Existing (No.)	Description	Comments
1	2	Pipe Culverts	Demolish and Replace on Section 1 Project Road.
2	9	Box Culverts	Demolish and Replace on Section 1 Project Road.
3	-	River Volta; River Alabo and Tributaries of River Lomen	Main Rivers Crossing Section 3 of the Project Roads.

Source: K E & T Design Document

#### 1.6.1.2 Traffic Studies of Sections 1 and 3 Catchment Zone

The Classified Manual Counts (MC) and Origin-Destination (OD) surveys were conducted on the respective road sections within the Sections 1 and 3 project roads catchment zone and shown in **Table 2.0**. This information was used in pavement design. The census locations are shown in **Figure 5**.

**Table 2.0 Traffic Studies and Results for Sections 1 and 3 Project Roads**

Serial Number	Road Section	ADT	AADT
1	Asutuare East – Asutuare Junction	550	528
2	Sege West – Battor	1,506	1,446
3	Sege North – Tema	5,612	5,387
4	Asutuare North – Kong	3,508	3,368
5	Juapong – Atimpoku	2,836	2,723
6	Bunso – Osino	8,740	8,827

Source: K E & T Design Documents



### 1.6.1.3 Geotechnical Investigations Results for Sections 1 and 3 of Project Roads

The subgrade and existing pavement conditions were investigated. The results were as follows:

- Subgrade CBR ranges from 8% to 20%
- A design CBR of 12% was used for pavement design

Black cotton clay was located at the following chainages and recommended to be removed to the stated depths below:

- CH. 10+800 – CH. 28+245 (1.5m deep)
- CH. 28+700 – CH. 48+000 (2.0 m deep)

### 1.6.1.4 Bridge Site Investigations for Sections 1 and 3 of Project Roads

The allowable bearing capacities and foundation depths are indicated below:

- Allowable Bearing Capacities obtained range from 200kPa to 290kPa
- Recommended Foundation depths range between 3.0m and 3.5m

### 1.6.1.5 Construction Materials Survey for Sections 1 and 3 of Project Roads

The construction materials survey and findings are shown in **Table 3.0**.

**Table 3.0 Construction Materials Surveys and Findings**

Serial Number	Construction Materials Type	Location Obtainable	Comments
1	Sand	Volta River at Asutware; Aveyime and Battor.	For Concrete Works
2	Rock Aggregates	Eastern Quarries; Bingleb Quarry; Rockshell Quarry and Mafi-Kpedzi Outcrop	For Concrete and Road Surfacing
3	Gravels	Afiencya and Its Environs.	Quality Gravel.

Source: K E & T Design Documents

### 1.6.2 Detailed Engineering Designs for Sections 1 and 3 of Project Roads

#### 1.6.2.1 Geometric Design for Sections 1 and 3 Project Roads

The detailed geometric engineering design for sections 1 and 3 of the project roads is shown in **Table 4.0**.

Table 4.0 Geometric Design for Sections 1 and 3 Project Roads

Design Parameters	Roads Classification	Design Speed	Minimum Radius (m)	Minimum Arc Length (m)	Minimum Curve (m)	Minimum K Value (Crest)	Minimum K Value (Sag)
-	National Road	100km/h	-	-	-	-	-
Horizontal Design	National Road	100km/h	694.495	170.0	-	-	-
Vertical Alignment	National Road	100km/h	-	-	85	64	28

Source: K E & T Design Documents

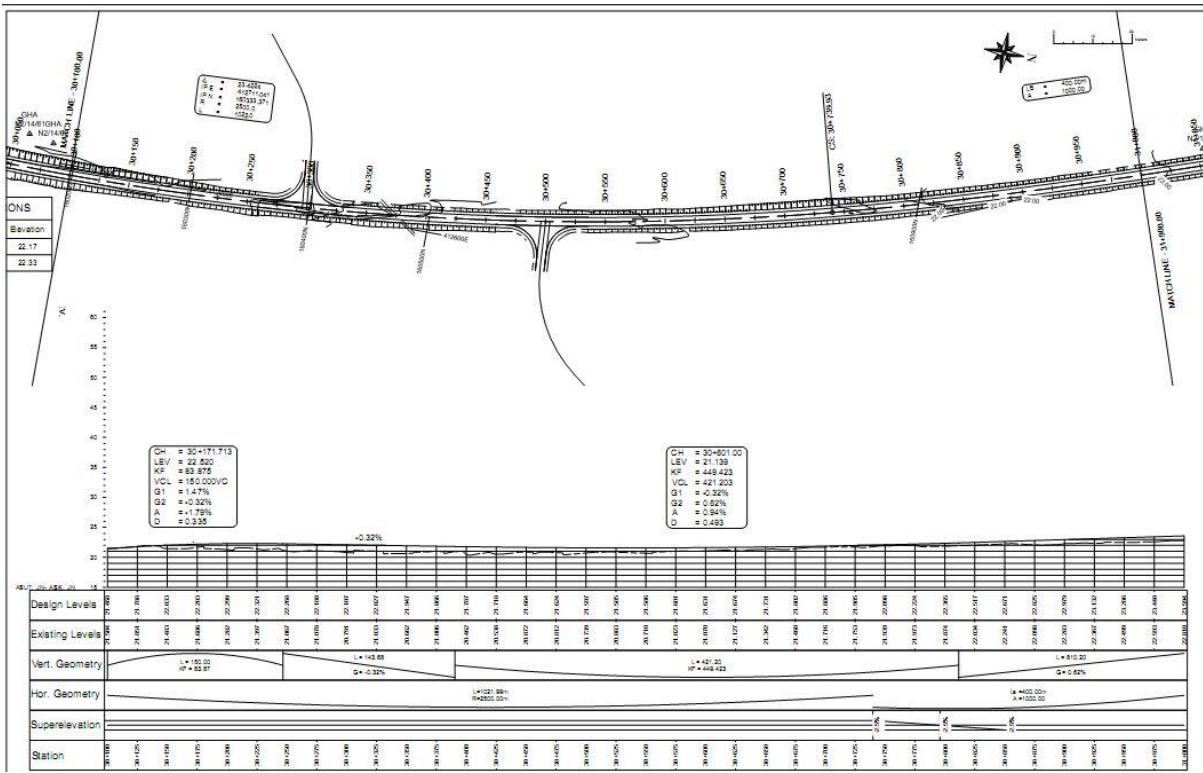
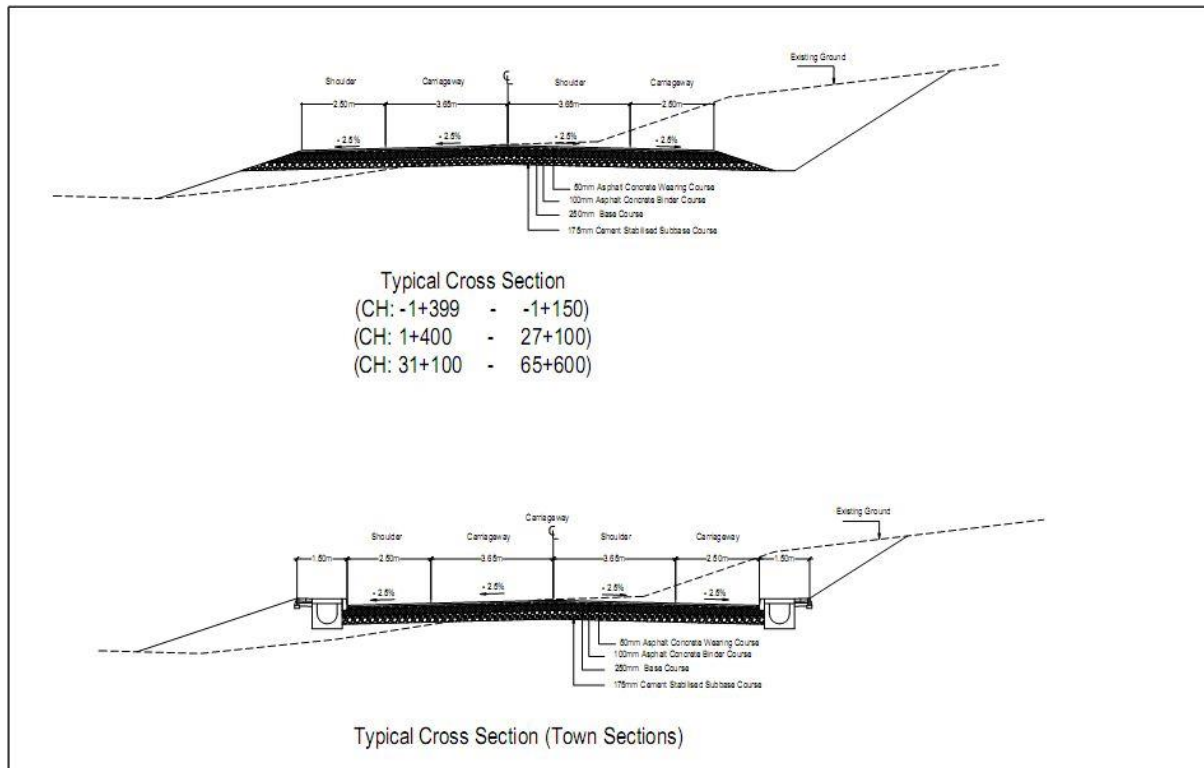
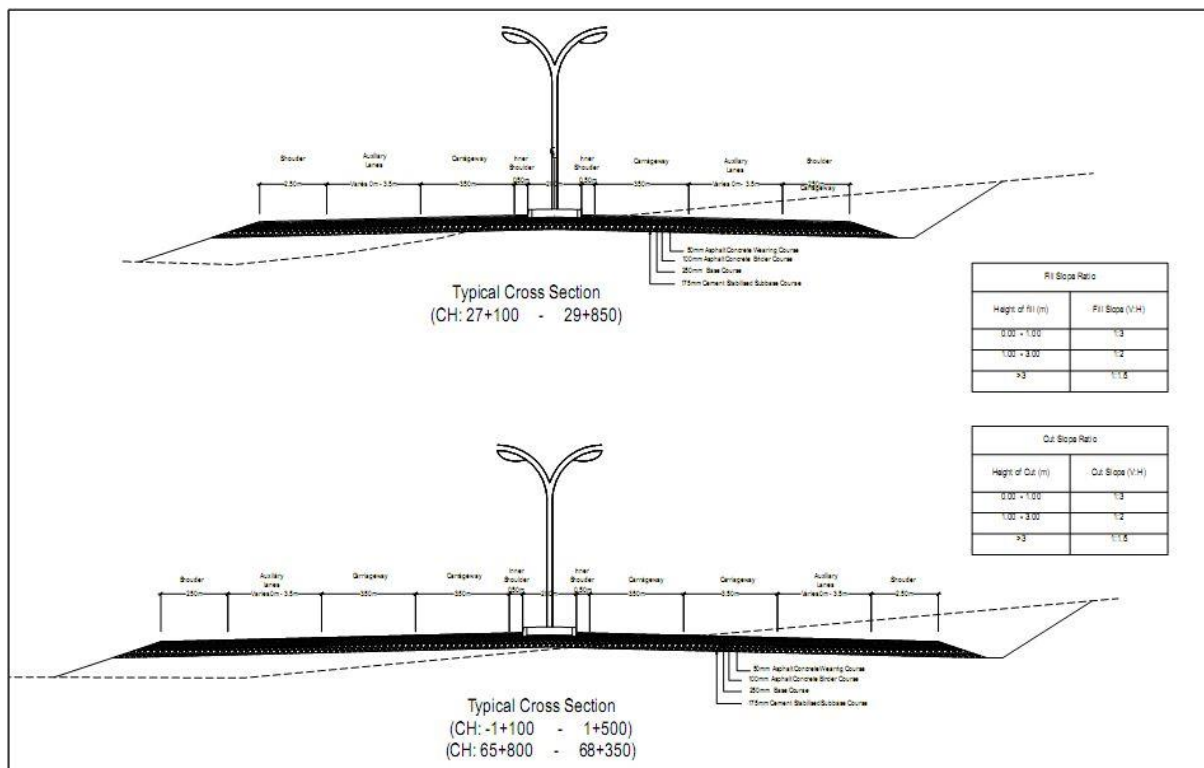


Figure 5: Sample Road Plan and Profile for Sections 1 and 3 Project Roads



**Figure 6: Typical Cross-Sections of Project Roads 1 & 3**



**Figure 7: Typical Cross-Sections of Project Roads 1 & 3**

### 1.6.2.2 Pavement Design for Sections 1 and 3 Project Roads

The following data was used for the pavement design of Sections 1 and 3 of the project roads:

- The subgrade strength (CBR) of 8%
- The cumulative number of equivalent standard axles (28,056,869) that will use project roads over the 20-year design life.

Pavement structure design utilized the following recommended data:

- 50mm asphaltic concrete wearing coarse
- 100mm asphaltic concrete binder
- 250mm crushed stone base
- 175mm granular sub-base

### 1.6.2.3 Drainage Design for Sections 1 and 3 Project Roads

The drainage design for hydrological structures for sections 1 and 3 of the project roads is shown in **Table 5.0**.

**Table 5.0 Drainage Design for Sections 1 and 3 of Project Roads**

Serial number	Chainage (km)	Structure Type	Span (m)	Comments
1	7 + 200	Bridge	15	Over River Dawhe for Section 1 Project Road
2	23 + 230	Bridge	45	Over Irrigation Canal for Section 1 Project Road
3	33 + 150	Bridge	15	Over Tributary of River Alabo for Section 3 Project Road
4	49 + 850	Bridge	15	Over Tributary of River Alabo for Section 3 Project Road
5	61 + 000	Bridge	60	Over River Alabo for Section 3 Project Road
6	63 + 025	Bridge	90	Over River Alabo for Section 3 Project Road
7	28 No.	Box Culverts	-	Selected locations for Sections 1 and 3 Project Roads
8	9 No.	Pipe Culverts	-	Selected locations for Sections 1 and 3 Project Roads

Source: K E & T Design Documents

### 1.6.3 Changes to JICA Design by Client

After presentation of the Interim Design Report by Consultant to the Client, the following design changes were made to incorporate the following features and new scope of works:

- Replacement of four (4No.) Rotary Roundabouts in the JICA design.
- Redesign and installation of four (4No.) Grade-Separation Interchanges.
- Design Consultants extra works in redesigning of four (4No.) Rotary Intersections into Interchanges through new topographic surveys, geotechnical investigation, geometric design, bridge design and cost estimation.
- Design Consultants undertook re-alignment of Section 1 of the project road around Golden Exotic Farms to avoid the Paddy Rice Farms and Irrigation Canals and provide a link to the farms.

The New Interchanges Designed are:

- Asutuare Junction Interchange at CH. 1 + 425 (Section 1 of Project Road)
- Volivo Interchange at CH. 29 + 095 (Section 1 of Project Road)
- Dufor Adidome Interchange at CH. 30 + 700 (Section 3 of Project Road)
- Asikuma Junction Interchange at CH. 68 + 625 (Section 3 of Project Road)

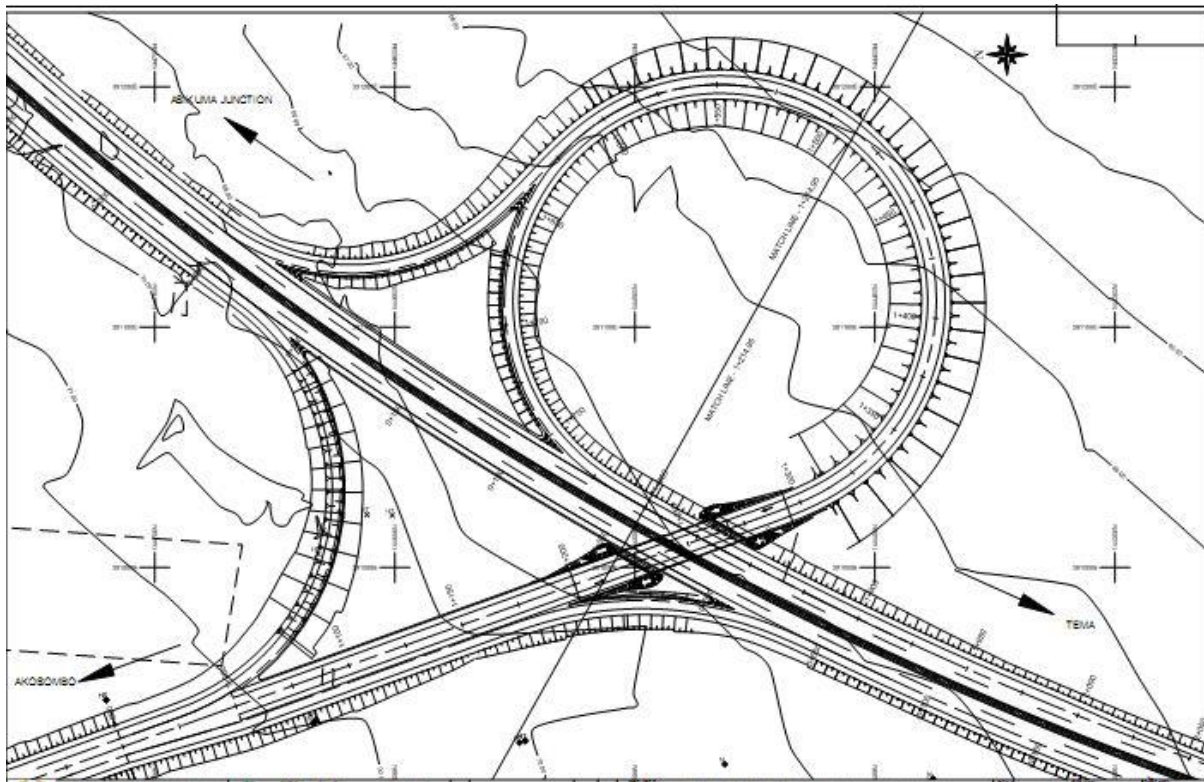


Figure 8: Layout of Asutuare Jn Interchange CH. 1+245

APPENDIX - B1 (CH 1+245)

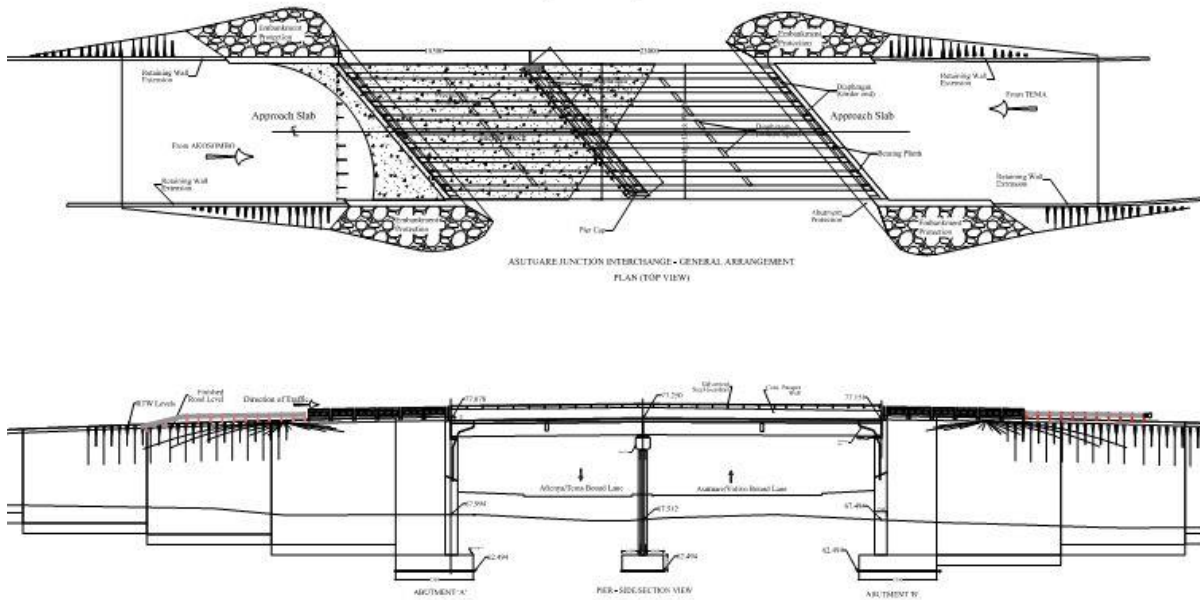


Figure 9: Bridge at CH. 1+245 (The Asutwae Interchange on Section 1 Project Road)

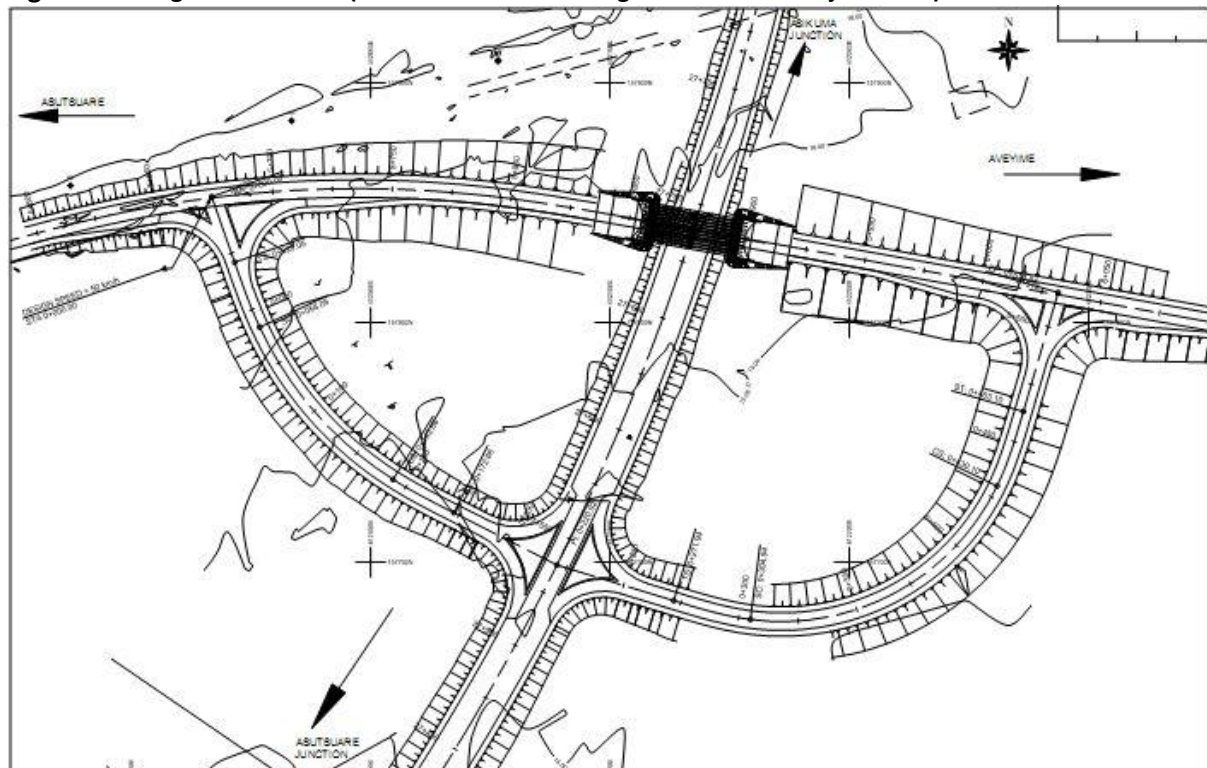


Figure 10: Layout of Volivo Interchange at CH. 29+350 on Section 1 Project Road



APPENDIX - B11 (CH 5+875)

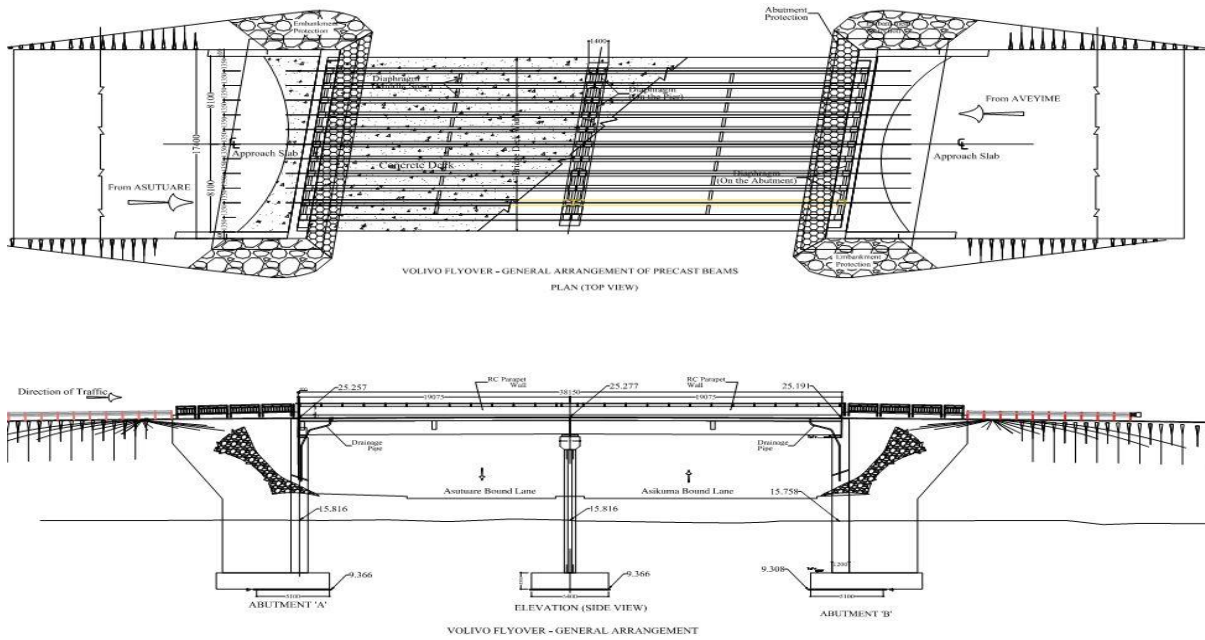


Figure 11: Bridge on Volivo Interchange at CH. 29+350 on Section 1 Project Road

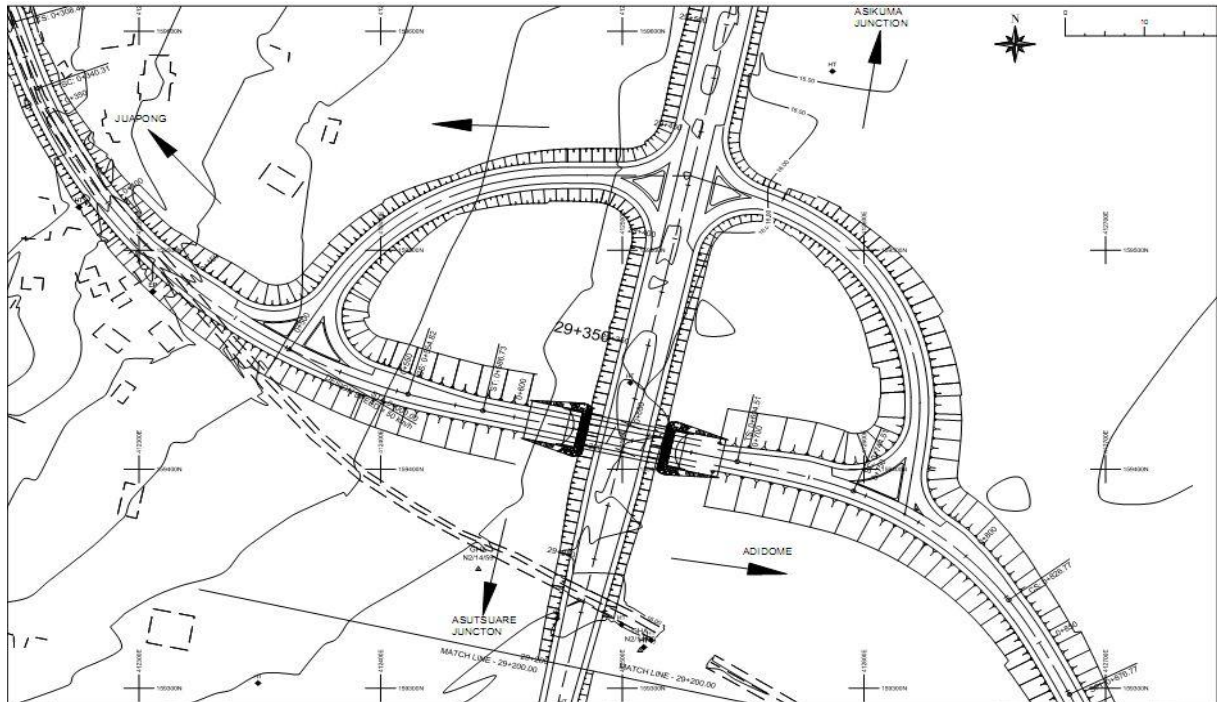


Figure 12: Layout of Dufor Adidome Interchange CH. 29+350 on Section 3 Project Road





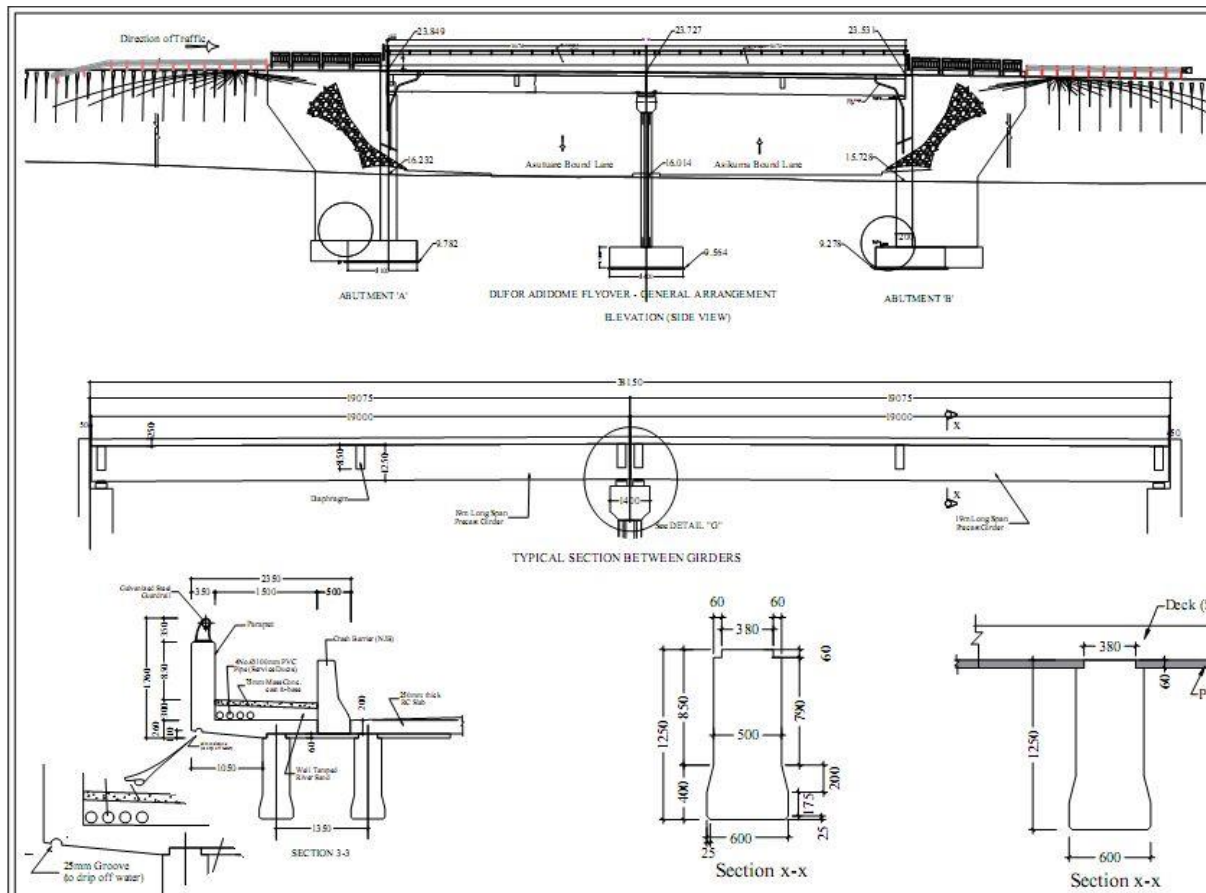


Figure 14: Bridge on Dufor Adidome Interchange CH. 29+350 on Section 3 Project Road

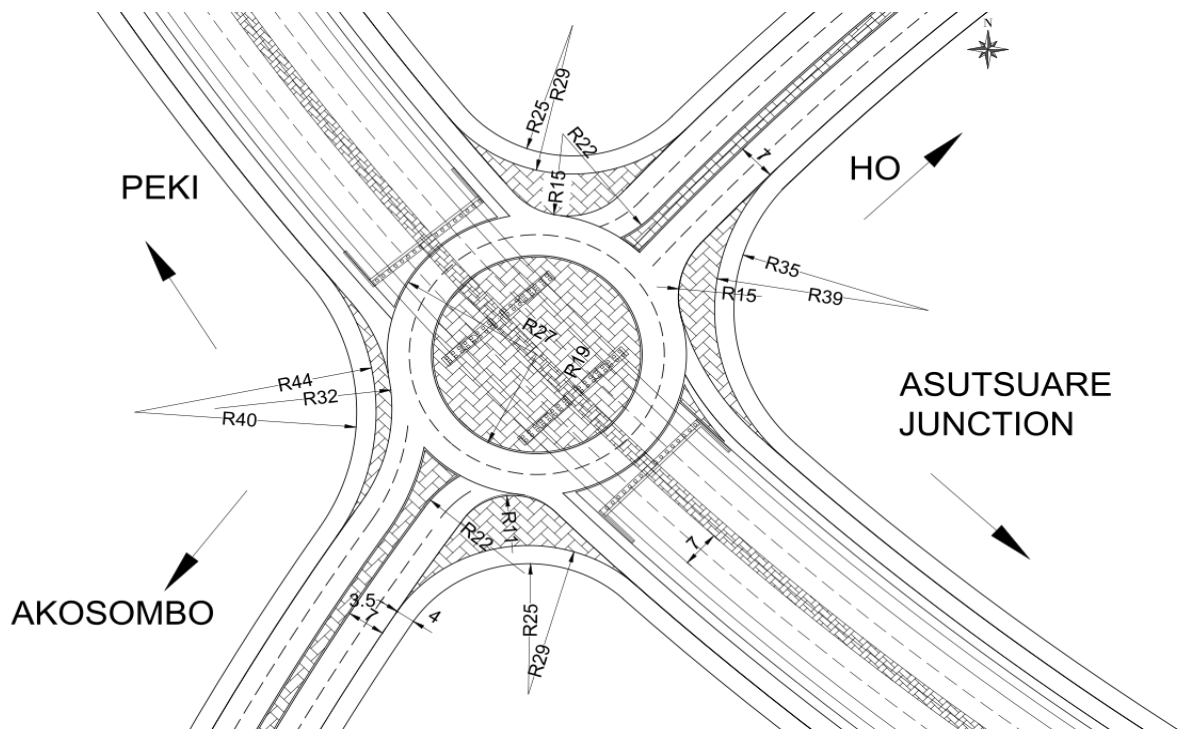
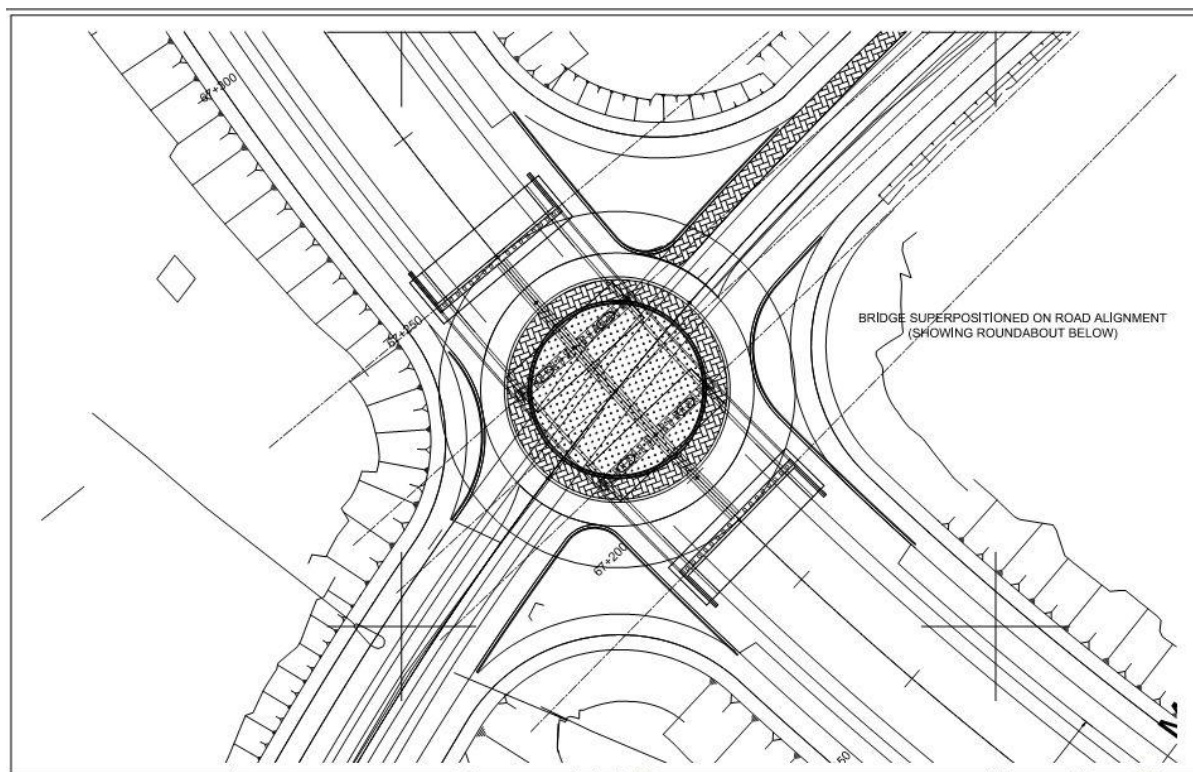


Figure 15: Layout of Asikuma Jn Interchange CH. 67+ 200 of Section 3 Project Road



**Figure 16: Bridge on Asikuma Jn Interchange CH. 67+ 200 for Section 3 Project Road**

#### 1.6.4 Additional Changes to Engineering Design by Client

The client made additional changes to the engineering design after presentation of the final design report with new cost estimates. Details of the new engineering design changes were as follows:

- Section 1 Project Road alignment be revised to avoid the black cotton clay section between CH. 11+700 – CH. 23+000
- The pavement design reviewed from asphaltic to double bituminous surfacing
- Drainage structure types be reviewed to save cost

##### 1.6.4.1 Revised Final Drainage Structures Design for Sections 1 and 3 Project Roads

The revised final drainage structures design for Sections 1 and 3 project roads is shown in **Table 6.0** at selected sections of project roads 1 and 3.

**Table 6.0 Revised Final Drainage Structures Design for Sections 1 and 3**

Serial number	Quantity	Pipe Culverts	Quantity	Box Culverts
1	7	1/0.9m Diameter	2	2/2.0m x 2.0m
2	3	2/0.9m Diameter	2	3/2.0m x 2.0m
3	9	1/1.2m Diameter	1	3/2.0m x 3.0m
4	3	2/1.2m Diameter	1	4/2.0m x 2.0m
5	8	2/1.8m Diameter	1	4/4.0m x 2.5m

Source: K E & T Design Documents

#### 1.6.4.2 Final Pavement Structures Design for Sections 1 and 3 Project Roads

The following recommendations have been made for Sections 1 and 3 project roads:

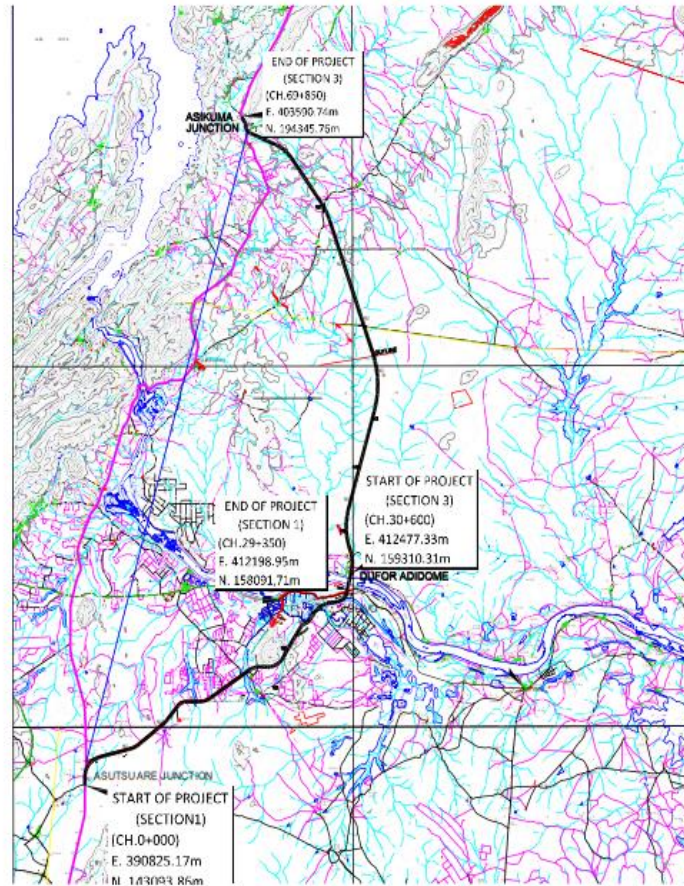
- Recommended for Section 1 (Asutuare Jn – Volivo Road):
  - ❖ Double Surface Dressing:
    - 150mm Mechanical Stabilized National Gravel Base Course
    - 200mm National Gravel Sub-base
    - 600mm Selected Fill
- Recommended for section 3 (Dufor Adidome – Asikuma Jn Road):
  - ❖ Double Surface Dressing:
    - 150mm Mechanical Stabilized National Gravel Base Course
    - 200mm National Gravel Sub-base
    - 600mm Selected Fill

#### 1.6.4.3 Final Bridges Structure Designs

The final bridges structure designs will be at the following chainages:

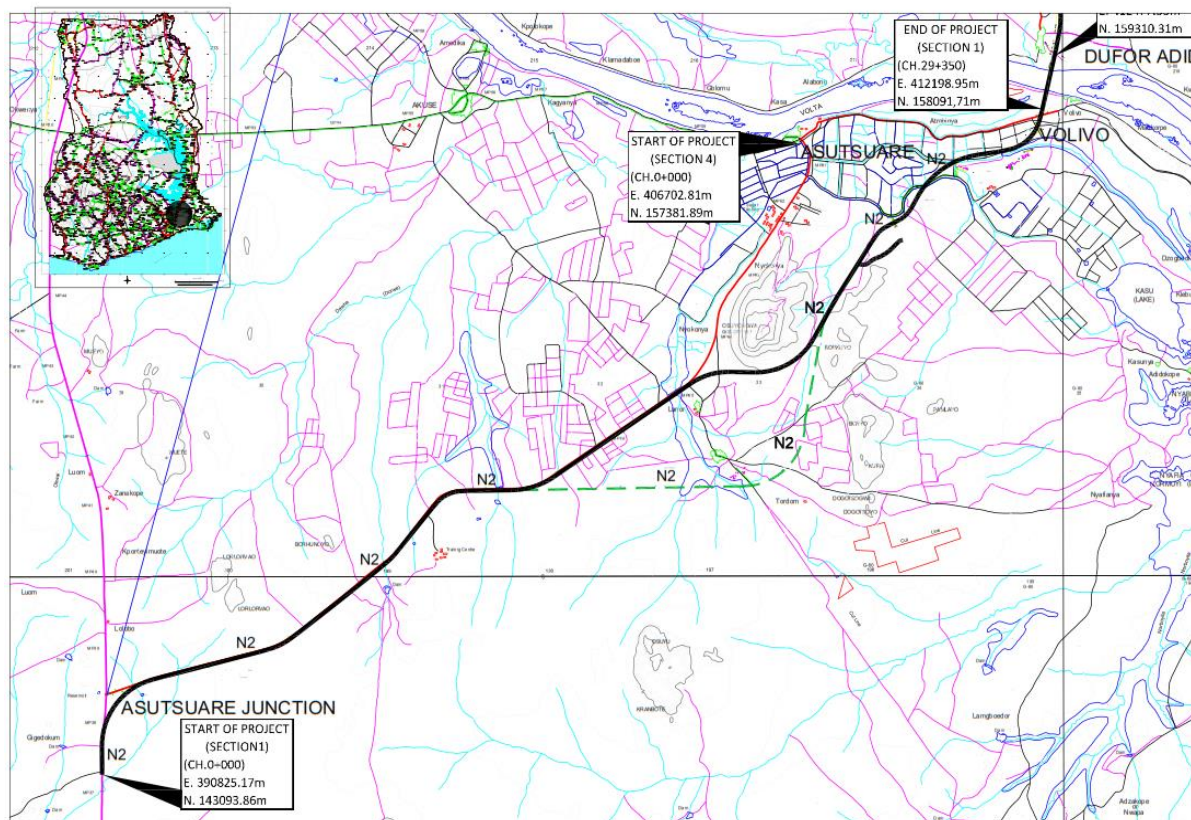
- CH. 8+600 – Over River Dawhe on Section 1 project road
- CH. 24+825 – Over Golden Exotic Irrigation Canal and Road on Section 1 project road
- CH. 25+850 – Over Irrigation Canal on Section 1 project road
- CH. 34+550 – Over Tributary of River Alabo on Section 3 project road
- CH. 51+242 – Over Tributary of River Alabo on Section 3 project road
- CH. 62+583 – Over River Alabo on Section 3 project road
- CH. 64+425 – Over River Alabo on Section 3 project road

The revised final design alignment maps are shown below:

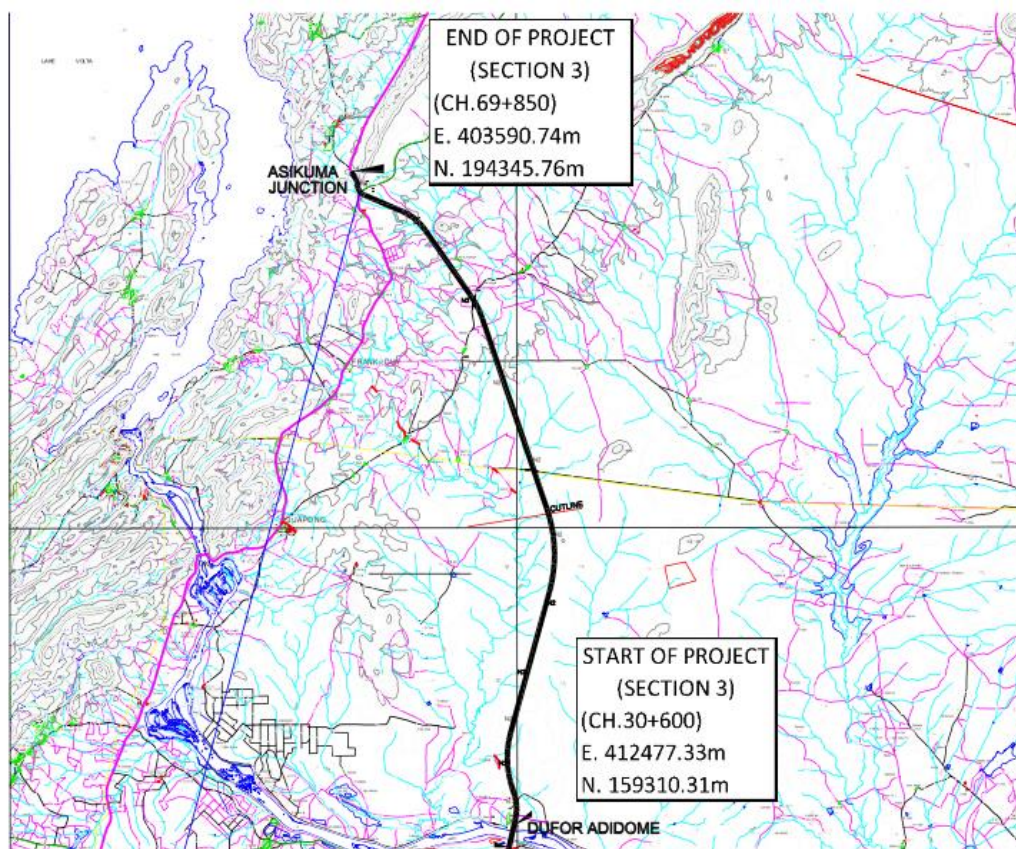


**Figure 17: Final Alignments for Sections 1 and 3 Project Roads**





**Figure 18: Final Alignment for Section 1 Project Road**



**Figure 19: Final Alignment for Section 3 Project Road**

#### 1.6.4.4. Revised Final Cost Estimate for Sections 1 and 3 Project Roads

The revised final cost estimate for Sections 1 and 3 project roads is indicated in **Table 7.0**

**Table 7.0 Final Cost Estimate for Section 1 and 3 Project Roads**

Project Road Section	Original Cost (USD)	Revised Final Cost (USD)	Difference in Cost (USD)
1	135,701,562.37	42,797,472.46	92,904,089.91
3	180,960,800.70	44,682,266.78	136,278,533.92
<b>Total</b>	<b>316,662,363.07</b>	<b>87,479,739.24</b>	<b>229,182,623.83</b>

Source: KE&T Design Documents

#### 1.6.5 Field Study Findings of Proposed Alignment (Section 2 Project Road)

- **Section 2: Asutuare – Aveyime Road (24.0km)**

##### 1.6.5.1 Hydrological/Drainage Structures Findings

The existing drainage structures are structurally weak and of inadequate hydraulic capacities. Moreover, such drainage structures would have to be replaced. See **Table 8.0** for existing drainage structures on Section 2 project road.

**Table 8.0 Hydrological/Drainage Structures Field Findings for Section 2 Project Road**

Serial Number	Number Existing (No.)	Description	Comments
1	14	Pipe Culverts	Demolish and replace
2	1	U-Culvert	Demolish and Replace
3	17	Box Culverts	Demolish and replace

Source: KE&T Design Documents

#### 1.6.5.2 Traffic and Geotechnical Investigations Results for Section 2 Project Road

The traffic studies and geotechnical investigations findings for Section 2 project road are disclosed below:

- ADT – 392
- AADT – 376
- An estimated Cumulative Equivalent Standard Axles (ESA) of 2,569,596 was obtained
- Sub-grade CBR obtained ranges from 9% to 32%
- A design CBR of 9% was used for pavement design

The same sources of construction materials as listed in Table 3.0 will also apply to the Section 2 project road.

#### 1.6.5.3 Bridge Site Studies, Findings and Recommendations for Section 2 Project Road

The bridge sites field studies findings and recommendations for allowable bearing capacity and depths of foundations are given below with the respective chainages.

- Allowable bearing capacity and depths of foundation are;
  - ❖ Bridge at CH. 0+575; 200kPa at depth 3.0m
  - ❖ Bridge at CH. 5+875; 200kPa at depth 3.5m

#### 1.6.5.4 Detailed Geometric Design for Section 2 Project Road

The detailed geometric design for Section 2 project road is shown in **Table 9.0**.

**Table 9.0 Detailed Geometric Design for Section 2 Project Road**

Design Parameters	Roads Classification	Design Speed	Minimum Radius (m)	Minimum Arc Length (m)	Minimum Curve (m)	Minimum K Value (Crest)	Minimum K Value (Sag)
-	Inter-Regional Road	80km/h	-	-	-	-	-
Horizontal Design	Inter-Regional Road	80km/h	424.413	140.0	-	-	-
Vertical Alignment	Inter-Regional Road	80km/h	-	-	70	30	18

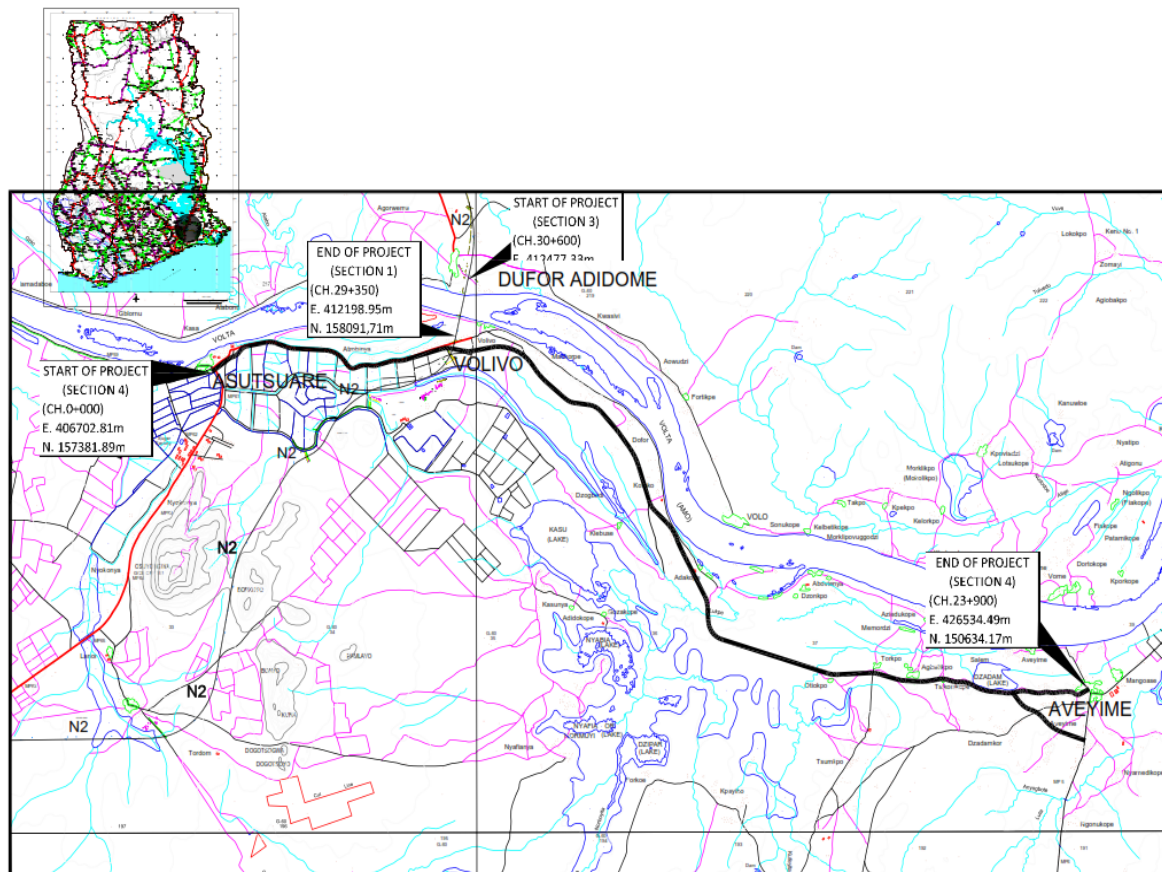


Figure 20: Asutware – Aveyime Road (Section 2 Project Road)



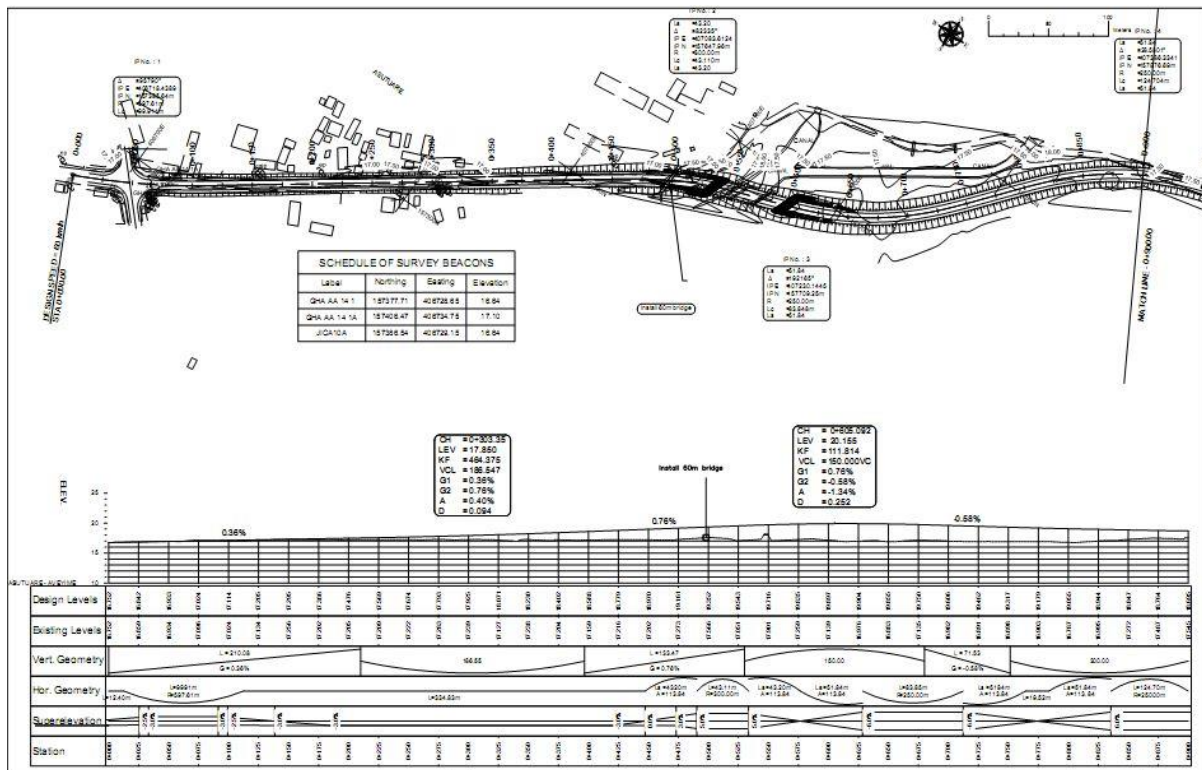


Figure 21: Sample Plan and Profile of Asutuaire – Aveyime Road (Section 2 Project Road)

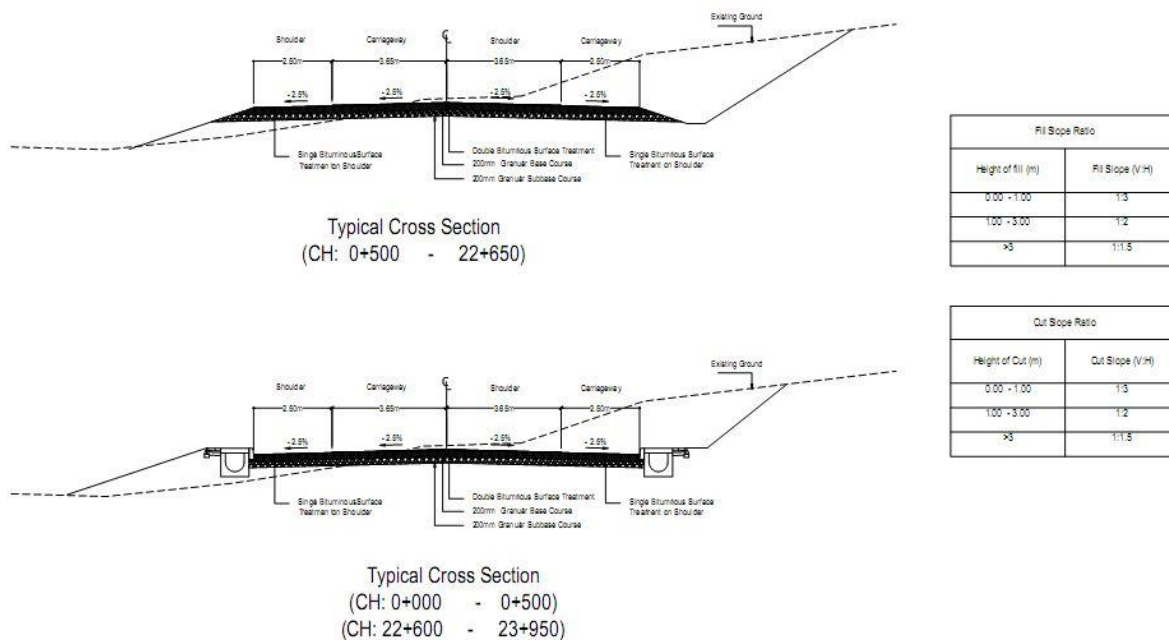


Figure 22: Typical Cross Section of Asutuaire – Aveyime Road (Section 2 Project Road)

#### 1.6.5.5 Final Pavement Design for Section 2 Project Road

The final pavement design for Asutuare – Aveyime Road (Section 2 Project Road) is depicted as follows:

- Double Surface Dressing:
  - ❖ 200mm Granular Road Base
  - ❖ 200mm Granular Sub-base

#### 1.6.5.6 Final Drainage Design for Section 2 Project Road

The final drainage design depicts the following:

- 3 No. Box Culverts
- 30 No. Pipe Culverts

#### 1.6.5.7 Final Bridges Design for Section 2 Project Road

The final bridges design depicts the following:

- 25m span at CH. 0+625 over the irrigation canal
- 15m span at CH. 0+800 over the irrigation canal

#### 1.6.5.8 Final Cost Estimate for Section 2 Project Road

The cost estimate for the Asutuare – Aveyime Road (Section 2 Project Road) is **USD 42,307,108.42**

### 1.7 Project Activities Description

The project activities to be undertaken have the over-riding consideration that there will be avoidance or preservation of environmentally sensitive areas and limitation of settlement structures demolition. In addition, there will be the minimization of the destruction of trees, crops and arable farmlands, where applicable. And the project activities phasing for consideration are: pre-construction, construction and post-construction (decommissioning, operation and maintenance) phases.

#### 1.7.1 Pre-Construction Phase Activities

The following pre-construction phase activities are required for the project: (i) feasibility study (ii) project roads identification and location (iii) project roads survey (iv) identification of site offices and site camps (v) acquisition of RoW (vi) consultations (vii) materials mobilization (viii) HIV/AIDS, Ebola and Cholera Awareness Training (ix) road safety awareness education (x) Environmental Protection Training.

**Feasibility Study:** This phase of the project cycle involves project planning and design, through conceptualization, feasibility survey and preparation of feasibility report. Various options consideration and recommendation are highlighted including the preferable option for the project.

**Project Roads Identification and Location:** The project roads identification and location were established from review of existing mapping. And field inspections were undertaken to confirm and clarify identified project roads and their exact locations.

**Project Roads Survey:** This activity was carried out by survey teams contracted by K E & T Consult Limited to carry out the survey of project roads, establish land profiles, limit of RoW and limit of construction and select the best alignments from several different options taking several factors into consideration. Some of the factors which were considered included, overall carriageway distances and the avoidance of the potential destruction of settlement structures (where applicable) and other environmentally and socially sensitive areas.

**Identification of Site Offices and Site Camps:** Principal site offices and camps will be selected with approval by the Site Engineer. This will also be done in direct consultation with the Metropolitan/District Assemblies and project community residents. Generally, the sites selection will involve fairly-flat terrain with sparse population densities and devoid of intense traffic and commercial activities.

**Acquisition of RoW (Right-of-Way):** Based on the issuance of an environmental permit from EPA Ghana, GHA will go ahead and acquire the RoW in compliance with all laws, regulations, operational directives and guidelines. Basically, the mode of assets valuation will take this dimension. Prior to the construction of the project roads, GHA and Land Valuation Division (LVD) will undertake a survey of all areas of land take, which includes a valuation of settlement properties, business structures, farmland, trees and crops in the RoW, that will have to be compensated. The valuation processes are discussed under compensation scheme.

**Consultations:** In the courses of the scoping exercise, some traditional authorities' representatives, project community residents, opinion leaders, metropolitan/district assembly's officials and regulatory agencies staff were consulted. This was to enable the survey team gain access to the proposed project roads, since the survey crew had to clear vegetation cover, trees and crops to make way for the survey program.

**Materials Mobilization:** The haulage of large quantities of construction materials-fine and aggregates-including cement by road to the project sites will take place. There will also be the transportation of heavy construction equipment and machinery to the project sites, under materials, machinery and equipment mobilization work program.

**HIV/AIDS, Ebola and Cholera Awareness Training:** HIV/AIDS, Ebola and Cholera Awareness Creation and Training Program will be organized for project communities and construction labor units. Moreover, sexually transmitted diseases awareness will be organized along- side the HIV/AIDS, Ebola and Cholera Awareness creation. The program will help explain the pandemic to the workers and the project community residents. The program is to make people aware that AIDS/Ebola and Cholera are real. Condoms and other accessories will be provided in the project catchment zones. There will also be the need to provide condoms at accessible points at the camp for the sexually active ones (ages 20-49) to use when the need arises. People will be encouraged to go for voluntary counseling and test at nearby hospitals or health centers.

**Road Safety Awareness Education:** Road Safety awareness Campaign through education will be instituted for project communities and construction crew to curb the likelihood of road accidents associated with project implementation. In collaboration with the National Road Safety Campaign of the Ministry of Roads

and Highways and the Traffic Police Unit, this activity will review the approach and methodology for the road safety awareness campaign and monitor the effectiveness of proposed mitigation measures. And the target groups will also include school children, street vendors and vehicle operators.

**Environmental Protection Training:** Environmental Protection Training will be organized for Contractors and other stakeholders. This activity will assist contractors to ensure environmental and social protection measures are adopted to instill good environmental and social management and monitoring practices during construction. Also, the training program will help protect, restore and enhance quality of the environment due to project implementation.

### 1.7.2 Construction Phase Activities

The following construction phase activities are required for the project: (i) excavations for drainage works (ii) excavations for Volta River suspension bridge anchorages (anchor blocks) foundations (iii) drainage works improvement for flood prone or low-lying areas (iv) construction of the Volta River suspension bridge structure (v) construction of roads protection structures (vi) improvements of sections of access roads leading into selected project roads (vii) installation of road signs and markings and relevant traffic control devices (viii) construction and/or treatment and sealing of shoulders or pavement works improvements (ix) construction and/or improvement of roads geometric features (x) construction and/or upgrading the existing project roads to asphaltic surface.

**Excavations for Drainage Works:** Information on drainage positions from design drawings will lead to excavation works for drains, culverts and storm channels. The drainage works will also lead to the casting of concrete or use of pre-cast concrete products such as U-drains, pipe culverts, box-culverts, etc., where applicable.

**Excavations for Volta River Suspension Bridge Anchorages Foundation:** The exact positions of the Volta River suspension bridge foundations will be derived from engineering design drawings. Such positions will lead to excavation works to hold anchorages foundations for reinforced concrete seat anchor blocks.

**Drainage Works Improvement for Flood Prone or Low-Lying Areas:** Flood-prone or low-lying sections of the project catchment zones will be improved through the installation of drainage channels or culverts to drain storm run-off. Such drainage works will involve the use of pre-cast concrete drain products or fresh casting of concrete batch in wooden or metallic molds.

**Construction of Volta River Suspension Bridge Structure:** The suspension bridge structure over the Volta River will be built of the following basic structural components: **anchorages** (massive concrete blocks which anchor main cables and act as end products of the suspension bridge); **main towers** (intermediate vertical structures which support main cables and transfer bridge loads to foundations); **main cables** ( a group of parallel-wire bundled cables which support stiffening girders/trusses by hanger ropes and transfer loads to towers); **stiffening girders/trusses** (longitudinal structures which support and distribute moving vehicle loads, act as chords for the lateral system and secure the aerodynamic stability of the structure).

**Construction of Roads Protection Structures:** Roads protection structures will be erected at sections with embankments or approach roads to suspension bridge structure. Such structures will take the form of stone pitching, dwarf walls or hydro-seeded surfaces. These structures serve as erosion protection schemes to aid preservation of embankment or landslide zones.

**Improvement of Sections of Access Roads Leading into Selected Project Roads:** Access roads leading to selected project roads will be improved of their geometric features to blend with the proposed project roads. Where applicable, access culverts will be erected to serve as drainage channels to prevent erosion of the project roads foundation.

**Installation of Road Signs and Markings and Relevant Traffic Control Devices:** To minimize traffic confusion and delays, road signs and markings will be installed at the appropriate sections of the project roads. Relevant traffic control devices like signalization will be provided, including traffic management facilities like walkways, crossing and public bus stops or laybys. These devices will aid free flow of vehicles.

**Construction and/or Treatment and Sealing of Shoulders or Pavement Works Improvement:** Pavement improvement and/or construction will be undertaken at pavement sections or shoulders with residual strength and conditions at the end of their design life. These works involve strengthening and/or widening. Where necessary, construction and/or treatment and sealing of shoulders will be carried out.

**Construction and/or Improvement of Roads Geometric Features:** The geometric features of project roads, where feasible, will be constructed and/or improved to meet current design standards. Some of the geometric features include a new carriageway suspension bridge over the Volta River at Volivo to improve the flow of traffic at that corridor and connect Dufor Adidome – Asikuma Junction Road (Section 3 Project Road).

**Construction and/or Upgrading the Existing Project Roads to Bituminous Surface:** Construction and/or surface improvement works of the project roads will lead to double bituminous surfacing of the selected project roads. Such surfacing scheme will prolong the design life of those project roads.

### 1.7.3 Post – Construction Phase Activities

The following post-construction phase (decommissioning, operation and maintenance) activities are required for the project: (i) decommissioning of site offices and lay-down areas (ii) drainage maintenance (iii) suspension bridge inspection and maintenance programs (iv) surface maintenance on paved roads (v) road side maintenance (v) road side furniture maintenance.

**Decommissioning of Site Offices and Lay-Down Areas:** (i) lay down areas materials, equipment and machinery decommissioning (ii) construction sites materials, equipment and machinery decommissioning (iii) decommissioned waste materials collections, stocking, haulage and transportation for reuse or disposal at approved locations (iv) sites restoration schemes involving re-vegetation and reclamation of areas. During decommissioning phase, all work areas and offices and workshops/garages and other temporary installations will be cleaned up and the site restored. These include removal or reuse of temporary buildings, materials, wood, refuse, surplus materials, embankments or another material that is not in the area before construction of works. All effected natural drainage systems will be restored, and excavated materials will be used to fill excavated areas. The damaged areas will be restored to make it compatible with future use.

**Drainage Maintenance:** The maintenance scheme for drainage systems will involve the following: ditch clearing by manual labor, ditch clearing by mechanical plant, re-excavation of damaged ditches to install high-capacity channels for storm run-offs, clearing and minor crack repair on drainage structures and erosion and scour repairs.

**Suspension Bridge Inspection and Maintenance Program:** The suspension bridge inspection program must be undertaken thoroughly as necessary to clearly establish its condition, inspected at the appropriate interval or frequency to insure its continued safe operation. Bridge Condition Assessment Investigations to be performed will include: initial inspections, routine inspections, fracture critical inspections, special inspections, in-depth inspections and damage inspections. However, the suspension bridge maintenance program will cover four (4) schemes: preventive maintenance, minor repairs, deferred maintenance and major repairs depending on the condition of the structures in an inventory.

**Surface Maintenance on Paved Roads:** Paved roads surface maintenance works will involve the following: pothole patching on bituminous surface dressing, pothole patching by pre-mix, pothole patching by hot-mixed asphaltic concrete, repair of depressions, ruts, shoving and corrugations, edge failure repairs on bituminous surface dressed roads and asphaltic concrete roads, crack sealing and resurfacing and re-sealing.

**Road Side Maintenance:** The roadside maintenance schedule will encompass the following scheme: grass cutting by manual labor, grass cutting by mechanical plant, tree/bamboo clearing and bush clearing.

**Road Side Furniture Maintenance:** The maintenance scheme of road side furniture will incorporate the following: road sign cleaning and painting, repair and replacement of traffic signs, repair and replacement of guide posts and guard rails and road line marking.

## 2 Potential Impacts

Fieldwork studies culminating in baseline information gathering and concerns derived from stakeholder's consultations and engagements, revealed potential impacts associated with the pre-construction, construction and operation phases of the project implementation program. Both beneficial and adverse impacts likely to be encountered during the project implementation processes are disclosed below.

### 2.1 Beneficial Impacts

The potential positive impacts of the proposal will be experienced on a long-term basis through induced characteristics. These induced transformation events are more socio-economic and cultural in nature. The potential key expected positive impacts of the proposals include the following: (i) the contribution to improvement of trade among project influence populations (ii) improved transport conditions of goods and populations in the relevant project roads sections (iii) the internal opening up and improved access to basic socio-economic infrastructure for the populations within the project roads enclave (iv) the creation of direct and indirect jobs and employment during the construction, operation and maintenance phases of the project roads implementation program (v) development of tourism potential (vi) improved and enhanced living conditions and social facilities for vulnerable populations served by the project roads (vii) reduced risks of landslides and erosion by strengthening and monitoring of slopes (viii) improved safety around schools, health centers, churches, mosques and other social amenities along the project roads catchment zones (ix) suppression of dust through bituminous surfacing of the project roads pavements (x) reduction in greenhouse gas (GHG) emissions (xi) reduction in travel time and costs (xii) reduced traffic congestion resulting from flooded road section conditions on the **Asutuare-Aveyime Road** during heavy precipitation conditions (xiii) improved travel safety and comfort (xiv) increased land values within the roads project enclave (xv) enhanced economy and gender development (xvi) improvement in agriculture, especially in rice, maize, cassava and vegetables cultivation using latest technology. Some key expected positive impacts have been discussed in the ESIA Report in Chapter 8 (Section 8.5).

### 2.2 Adverse Impacts

This has been disclosed in detail in the ESIA Report in Chapter 8 (Section 8.2) under pre-construction, construction and operation phase. The section highlights the following: impact analysis; potential negative impacts for the roads project consideration under **pre-construction phase** (survey works; work camp and site offices; removal of trees; demolition of structures, air quality, human health and safety); **construction and operation phases** (air quality; soundscape and vibration; borrow pits generation; soil erosion and sedimentation; waste generation and removal; water quality and supplies; concrete batch plant waste; construction camps and site offices; biodiversity; traffic and transportation; human health and safety; visual quality and amenities; history, archaeology, heritage resources; pedestrian-vehicular conflicts; public utilities and services; hydrology and drainage systems; project communities access; loss of assets and compensation payment; influx population surge; greenhouse gas (GHG) emissions). Climate change and complimentary initiatives, including gender issues have been discussed in detail under the **operation phase**.

Moreover, the potential impacts will primarily emanate from land acquisition for the purposes of the project through claiming the right-of-way (RoW) to the required standard to accommodate the respective project roads sections (1, 3 and 2), including shoulders, provision of additional climbing lanes, bus-bays, interchanges and service roads. There will be the creation of detours, access roads to material

sources and acquisition of land for establishing borrow pits. The project roads designs for sections 1, 3 and 2 indicate that all the requirements for the highways will be confined to the standard design for the RoW to minimize potential environmental and social impacts. However, due to the additional land take required to accommodate the widened project roads, with regards to grade separated interchanges and their associated service roads, various assets such as buildings/structures, crops/farms and bare land (fallow lands) will be affected. Summary of affected assets types and number of PAPs are indicated in Tables 10.0, 11.0 and 12.0 for roads project sections 1, 3 and 2 respectively.

**Table 10.0 Summary of Affected Assets and Number of PAPs (Section 1: Project Road)**

Serial Number	Name of Project Road	Description of Affected Assets	Number of PAPs	Comments
1	Asutuare Junction – Volivo Road (28.7 km)	Buildings/ Structures	134	Compensation values have been assessed.
2	✓	Crops/Farms	278	Compensation values have been assessed.
3	✓	Bare Land (472.67 acres)	295	Compensation values have been assessed.
4	✓	<b>Sub-Total</b>	<b>707</b>	-

Source: GHA Data Records

**Table 11.0 Summary of Affected Assets and Number of PAPs (Section 3: Project Road)**

Serial Number	Name of Project Road	Description of Affected Assets	Number of PAPs	Comments
1	Duffor Adidome – Asikuma Junction Road (38.4 km)	Buildings/ Structures	60	Compensation values have been assessed.
2	✓	Crops/Farms	120	Compensation values have been assessed.
3	✓	Bare Land (532.13 acres)	134	Compensation values have been assessed.
4	✓	<b>Sub-Total</b>	<b>314</b>	-

Source: GHA Data Records

**Table 12.0 Summary of Affected Assets and Number of PAPs (Section 2: Project Road)**

Serial Number	Name of Project Road	Description of Affected Assets	Number of PAPs	Comments
1	Asutuare – Aveyime Road (24.0 km)	Buildings/ Structures	204	Compensation values have been assessed.
2	✓	Crops/Farms	215	Compensation values have been assessed.



3	✓	Bare Land (532.13 acres)	35	Compensation values have been assessed.
4	✓	<b>Sub-Total</b>	<b>454</b>	-

Source: GHA Data Records

### 3 Organizational Responsibilities

**Introduction:** Specific institutions have been identified to participate in the implementation of the resettlement/displacement process. The ultimate responsibility, however, will rest with the GHA. For the purpose of the RAP, the activities of the major institutions have been highlighted. The most likely agencies are: (i) Ghana Highway Authority (GHA) (ii) Ministry of Transport (MoT) (iii) Land Valuation Division (LVD) (iv) Ministry of Finance and Economic Planning (v) Ministry of Local Government and Rural Development (vi) Lands Commission (vii) Town and Country Planning Department (viii) Environmental Protection Agency (EPA) (ix) Attorney General's Department (x) District/Metropolitan Assemblies (xi) Utilities Agencies/Companies.

**Ghana Highway Authority (GHA):** The Ghana Highway Authority is one of the agencies of the Ministry of Roads and Highway (MRH). The GHA is a semi-autonomous body with a responsibility for the provision and management of trunk roads. It was originally established in 1974 as the organization responsible for the development and administration of the entire national road network. Since the GHA Act of December 1997 however, its role has been limited to the administration, control, development and maintenance of trunk roads and related facilities subject to the policies of the MRH. The roads project falls under the jurisdiction of GHA that will also serve as the resettlement implementation agency and will have ultimate control of the project roads.

**Ministry of Transport (MoT):** The Ministry of Transport (MoT) is the ministry responsible for road transport in Ghana. It is the duty of the MoT to ensure that this RAP is carried out effectively, in line with its 2010 Resettlement Policy Framework. The ministry will ensure that funds required for the implementation of this RAP are made available.

**Land Valuation Division (LVD):** The Land Valuation Division (formerly Land Valuation Board (LVB)) was formally set up in 1986 to perform functions related to valuation of various properties for specified purposes. The LVD is accordingly the statutory agency responsible for the processing of compensation claims on compulsory acquisitions. LVD will receive and verify documentation on affected properties. This is to ensure that payments are not made to people who are not adversely affected. Moreover, LVD is to ensure that compensations offered affected persons are reasonable and adequate.

**Ministry of Finance and Economic Planning (MoFEP):** The Ministry of Finance and Economic Planning is the agency that manages the central government's budget. The Finance Ministry is responsible for releasing money to be paid to PAPs by GHA under this Proposal. Upon receipt of the approval of the compensation figures, this ministry, subject to the availability of funds releases the total amount to GHA for subsequent payment to the PAPs.

**Ministry of Local Government and Rural Development (MoLGRD):** The District and Municipal Assemblies, under the Ministry of Local Government and Rural Development will be part of the implementing agencies which will participate in the planning, implementation and monitoring of the resettlement/compensation scheme.

**Land Commission (LC):** This is the state agency charged primarily with the management and administration of state and vested lands. It is responsible for advising on policy framework for development of areas to ensure that development of such areas is coordinated. The functions of the commission are spelt out in Article 258 of the 1992 Constitution and the Lands Commission Act (Act 483)

1994. The Commission's role in the area of compulsory acquisition is that it serves as a member/secretary to the site selection request for compulsory acquisition by GHA and recommends its acceptance or otherwise. The proprietary plan covering the site to be acquired is plotted by the Commission for the approval of the minister responsible for lands, before an executive instrument would be issued and gazette.

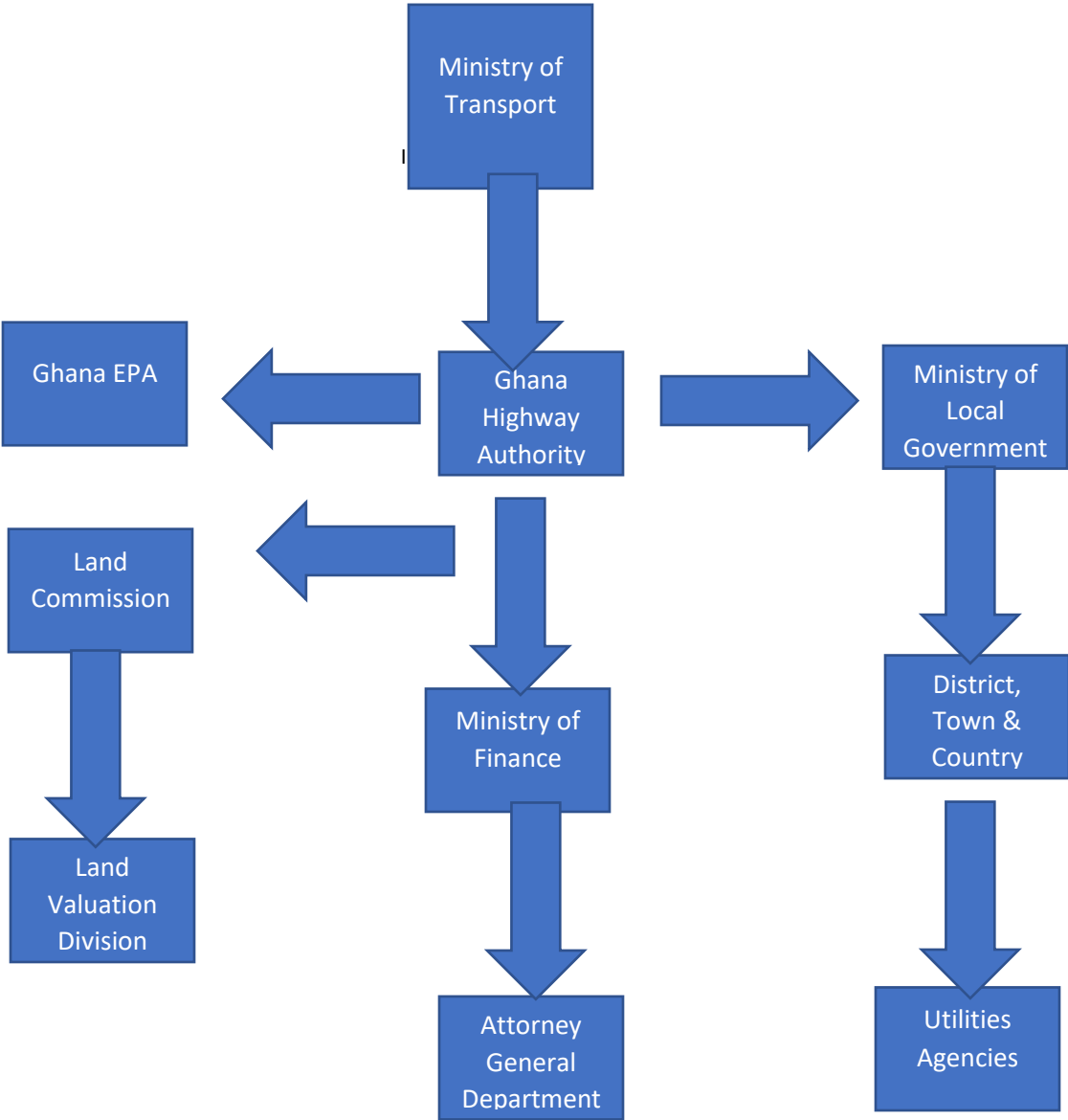
**Town and Country Planning Department (TCPD):** The Department was set up to ensure that developments are done orderly and that land use is maximized. It would be responsible for the examination/auditing of layouts for the project roads, vetting and approval of the prospective layouts and would cross-check all reservation based on projected land use plans. The TCPD would issue permit for approval of the roads project to be implemented. And the current local government system has placed the Town and Country Planning under the District, Municipal and Metropolitan Assemblies. Finally, the TCPD would ensure that the RoW is implemented as per the approved planning schemes on each project road.

**Environmental Protection Agency (EPA):** The EPA was established by Environmental Protection Agency Act 1994 (Act 490). The EPA was charged with the duty of prescribing standards and guidelines relating to environmental protection and/or pollution. The Agency may by notice in writing direct any developer carrying on any project to submit an Environmental Impact Assessment covering the roads project. The Environmental Assessment Regulation 1999 has listed the developments that require clearance with the EPA. Development of road network is one of the undertakings that require the issuance of environmental permit before construction can be done. For this roads project EPA will issue a clearance for the Proposal and monitor the resettlement/rehabilitation operations.

**Attorney General's Department:** The Attorney General's Department has redress mechanisms in place for aggrieved persons. Affected persons who are not satisfied with compensation due them are empowered by the constitution to seek redress in the court of law. When this happens, the Attorney General's Department represents the government in the court's proceedings. The Attorney General's department is also responsible for drafting the Executive Instrument (EI) for acquiring the needed land for the roads project.

**Utilities Agencies:** The utility companies that are likely to play a role in the resettlement schemes are the following: Electricity Company of Ghana (ECG); Ghana Water Company Limited (GWCL) and Mobile Phone Companies. These agencies at the appropriate times will disconnect and reconnect PAPs to their services before and after relocation. Special consideration has been given to PAPs so that they are not treated as usual applicants for services to their new places.

Organizational Responsibilities Chart with Schematic Relationships



**Introduction:** The effectiveness of resettlement planning requires continuous participation and thorough engagements and consultations with a wide range of project affected persons and stakeholders in the project zone of influence. These include individuals or groups who will be positively or negatively affected by the project including potential host communities. Comprehensive engagements and consultations have been held with various relevant stakeholders and the project affected persons from the reconnaissance stage through the public consultations, the project affected persons census along the project roads, a socio-economic survey and an assets inventory.

**Relevant Stakeholders Consultations and Engagement:** A combined public, community and relevant stakeholder's consultations and engagements were undertaken between 18<sup>th</sup> October and 6<sup>th</sup> December 2014 within the project communities. The issue areas of interest centered on the following: traffic congestion; traffic flow disruption and severance effects; availability of money for the project; deeper consultations with project communities; pedestrian walk-way provision; demolition of structures; destruction of paddy rice farms and the fate of rice farmers in the acquisition of new rice farmlands; payment of compensation. After the primary consultations and engagements of project community residents, secondary stage relevant stakeholder consultations and engagements for the proposed roads project were conducted to capture the significant concerns associated with the project from all concerned and interested parties. The process ensured that these groups were given the platform to voice their opinions and views. The process also gave prior disclosure of relevant and adequate project information to stakeholders. The KE&T ESIA/RAP Study Teams held six (6) public meetings on 18/10/2014, 05/12/2014, 06/12/2014, 26/06/2015, 27/06/2015 and 13/11/2015 along the roads project enclave during the development of the ESIA and RAP Reports. Moreover, the KE&T ESIA Study Team, GHA and AfDB Project Design Teams held Stakeholders Community Needs Assessment public meetings on 18/04/2018 at Asutuare Junction, Asutuare, Volivo, Aveyime and Dofor Adidome along the project roads corridor. Finally, KE&T ESIA/RAP Study Teams, GHA RAP and AfDB ESIA/RAP Teams held Stakeholders ESIA/RAP Awareness Education public meetings on 20/04/2018 at Asutuare Junction, Asutuare, Volivo and Aveyime. The same program was slated to have been held the following day (21/04/2018) at Dofor Adidome but due to funeral celebration, this program could not come on since the project community residents attended the funeral celebration. However, the rescheduled program without the AfDB ESIA/RAP Team and GHA RAP Team came off on 25/05/2018 which was undertaken by the KE&T ESIA/RAP Study Team. The absence of the AfDB ESIA/RAP and GHA RAP Teams was due to scheduling plans and programs conflict.

The participants of the various public meetings included representatives of community-based associations such as the Ghana Private Road Transport Union (GPRTU), Drivers, Taxi Owners, Market Women, Rice Traders, Teachers, Parents, Home Owners, Property Developers, Nurses, Assembly Men and Women, Elders, Rice Farmers, Fruits and Vegetable Farmers, Project-Affected-Persons (PAPs) and Project Community Residents. The stakeholder consultative and engagement meetings provided views, opinions and suggestions on the most appropriate considerations on the construction and use of the proposed roads project. The sessions also laid out fears and concerns to be addressed during construction. To ensure that both women's and men's views were taken on board in the project design, the public consultations were carried out in an all-inclusive manner. The consultations sought community participation and instant feedback into the project design especially related to matters of road alignment, RoW and shoulders design, resettlement and compensation payment. The consultations created awareness and identified positive and negative socio-economic impacts of the roads project, proposed

mitigation measures to address the potential impacts during pre-construction, construction and operation phases programs,

The following are some of the issues and concerns raised at the stakeholder consultative and engagement meetings which were responded to and as much as possible recommendations made have been incorporated in project design: (i) road safety measures for school children (ii) public appreciation and concerns of the roads project due to their importance (iii) high expectations (direct and indirect jobs creation and employment generation from the project, development and improvement of businesses, provision and enhancement of access to social amenities, reduction of travel time) (iv) fears and concerns associated with the roads project including increased road accidents and increased HIV/AIDS/STDs (v) timely and fair compensation payments (vi) road drainage and flooding especially on Asutuare-Aveyime Road and Dofor Adidome-Asikuma Junction low-lying wet land sections (vii) establishment of toll plaza (viii) waste generation and disposal along the project roads corridor (ix) traffic management and security provisions for road users during construction (x) establishment of a Grievance Redress Mechanism for PAPs and all road users and the need for continuous information sharing (xi) noise and excessive vibrations during construction (xii) public infrastructure and utilities disturbance or destruction repairs during construction works (xiii) compensation payment to restore project community livelihood (xiv) PAPs education on the use of compensation money (xv) timing of compensation payment (xvi) establishment of appropriate and accessible grievance mechanisms (xvi) identification and assistance to vulnerable persons and households

**Summary of Stakeholder Community Needs Assessment:** The AfDB Project Design Team mission was to visit the project roads corridor consult and interact with the affected project populations and assess the community needs. Such needs will be prioritized and factored into the project budgetary provisions for approval by the board. Moreover, the AfDB Project Design Team wanted to know and assess how the project will affect the communities, livelihoods and future planned programs. The summary of stakeholder community needs assessment is detailed in Table 13.0.0.

**Table 13.0 Summary of Stakeholder Community Needs Assessment**

Serial Number	Project Location	Community Needs	Commentary
1	Asutuare Junction	CHPS compound facility, community center, JHS/SHS School, Borehole, pipe borne water supply, public toilet, market, timely PAPs compensation payment, employment priority for youth.	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design Team, GHA and AfDB Project Design Teams and Community Residents.
2	Asautuare	New Market, pipe borne water, public toilet, employment priority for youth, upgrade lorry park, timely PAPs compensation payment.	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design Team, GHA and AfDB Project Design Teams and Community Residents.
3	Volivo	Completion of CHPS compound facility, pipe borne water, public toilet. Jobs and employment for	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design

		youth, market, business assistance loans for traders, community center.	Team, GHA and AfDB Project Design Teams and Community Residents.
4	Aveyime	Clinic, pipe borne water, ICT center (present in dilapidated building), community center, jobs and employment for youth, gari processing factory.	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design Team, GHA and AfDB Project Design Teams and Community Residents.
5	Dofor Adidome	Pipe borne water, dams for animals and people, hospital, market, technical-vocational school, ICT laboratory, public toilet, teacher's bungalow and nurse's accommodation, employment for youth, nursery school, PAPs compensation prompt payment, police quarters for project personnel protection.	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design Team, GHA and AfDB Project Design Teams and Community Residents.
6	Asikuma Junction	Pipe borne water, market, public toilet JHS/SHS, technical and vocational school, clinic, PAPs compensation payment, jobs and employment for youth.	Public meeting held on 18/04/2018. Attendance by KE&T ESIA/RAP/Project Design Team, GHA and AfDB Project Design Teams and Community Residents.

Source: KE&T Data Records

**Stakeholder Community ESIA/RAP Reports Awareness Education:** The AfDB ESIA/RAP Team mission was to visit the project roads corridor consult and interact with the affected project populations and educate the community on the provisions of the ESIA and RAP Reports. Such provisions dealt with project pollution concerns and controls which have been factored into the project budgetary provisions for approval by the board. The AfDB ESIA/RAP Team wanted to know and assess how the PAPs are dealing with the project displacement of assets and properties impinged upon by the roads project alignments. Moreover, the team wanted to assess the effects of the project roads on the communities, livelihoods and future planned programs. The summary of stakeholder community ESIA/RAP Reports Awareness Education is shown in Table 14.0.

**Table 14.0 Summary of Stakeholder Community ESIA/RAP Reports Awareness Education**

Serial Number	Project Location	ESIA/RAP Reports Awareness Education	Commentary
1	Asutuare Junction	<b>Appreciation:</b> improved roads condition; improved transportation;	Public meeting held on 20/04/2018. Attendance by KE&T ESIA/RAP Team, GHA and AfDB ESIA/RAP

		<p>increased social and economic interaction; increased commercial activities.</p> <p><b>Concerns:</b> air pollution; noise pollution; pollution of water resources; destruction of natural vegetation; disturbance of natural habitat of wildlife; Location of borrow pits; pedestrian consideration; project compatibility with general planning schemes and adjoining land uses; compensation payment options; compensation for land and crops; compensation schedule; project affected land development prior to compensation payment implementation.</p>	Teams and Community Residents.
2	Asautuare	<p><b>Appreciation:</b> improved roads condition; improved accessibility to natural resources; enhanced socio-economic development.</p> <p><b>Concerns:</b> adequate consultation with relevant stakeholders; proper location of borrow areas and their reinstatement; water pollution; construction traffic and safety; noise and air pollution; inadequate; compensation for PAPs; disturbance to businesses; restoration of livelihood; cash versus in-kind compensation options.</p>	Public meeting held on 20/04/2018. Attendance by KE&T ESIA/RAP Team, GHA and AfDB ESIA/RAP Teams and Community Residents.
3	Volivo	<p><b>Appreciation:</b> development of their respective communities; creation of employment opportunities; increase commercial activities;</p>	Public meeting held on 20/04/2018. Attendance by KE&T ESIA/RAP Team, GHA and AfDB ESIA/RAP Teams and Community Residents.



		<p>improved roads condition.</p> <p><b>Concerns:</b> development of their respective communities; creation of employment opportunities; increase commercial activities. Improved roads condition; assets affected by RoW; assurance of compensation payment; protection of vulnerable groups and levels of marginalization or disadvantage of others.</p>	
4	Aveyime	<p><b>Appreciation:</b> Road Sector Development Program attainment; socio-economic development opportunities; traffic congestion removal; improved traffic flow; enhanced vehicular availability.</p> <p><b>Concerns:</b> construction site camps/offices location; Settlement farmlands destruction; water resources pollution; noise and dust pollution; relocation of affected utilities; envisaged changes in project roads alignments effect on land uses; implication of cut-off date; general procedures for compensation payment; assessment and evaluation of affected assets for compensation payment.</p>	Public meeting held on 20/04/2018. Attendance by KE&T ESIA/RAP Team, GHA and AfDB ESIA/RAP Teams and Community Residents.
5	Dofor Adidome	<p><b>Appreciation:</b> Easy transportation; cheaply acquired goods accessibility; development of towns and villages along the roads; population</p>	Public meeting held on 20/04/2018. Attendance by KE&T ESIA/RAP Team, GHA and AfDB ESIA/RAP Teams and Community Residents.

		<p>increase along project roads; increase and access to social amenities provision.</p> <p><b>Concerns:</b> Increase in HIV/AIDS/STDs infections; loss of family values; properties destruction; destruction of wildlife habitats; accidents from speeding vehicles; project effect on people livelihood; timely payment of compensation; cash compensation payment preference or option; agreement on definitive compensation and assistance measures.</p>	
6	Asikuma Junction	<p><b>Appreciation:</b> Expected improvement in the standard of living for most roadside communities; expected enhancement in the mobility of project community residents; unemployment and poverty reduction rates reduction anticipation; enhancement of local and regional access; reduction of vehicle operation cost and travel time.</p> <p><b>Concerns:</b> traffic accidents involving humans and livestock; dust and noise impacts; anticipated increased pressure on public services; late payment of compensation to PAPs; unavailability of money for the project; demolition of structures; compensation schedule.</p>	Public meeting held on 25/05/2018. Attendance by KE&T ESIA/RAP Team and Community Residents.

Source: KE&T Data Records

**Demonstration of Broad Community Support:** This was achieved through very deepened and comprehensive consultations and engagement with various stakeholders from the reconnaissance stage

through the conduction of baseline socio-cultural and socio-economic household surveys. During the household survey stage, the data was cited for determination of the potential project affected persons within the project roads corridor. The consultations and engagement processes were tailored to seek the views of the various stakeholders within the project roads corridor and the public on the different stages and aspects of the designs. Moreover, the public consultations and engagements for a served to create awareness on how the project roads program will impact on their daily lives during and after the implementation stage.

Prior to the conduction of the socio-cultural and socio-economic surveys, meetings were held with all the relevant stakeholders, including district/municipal assembly officials and opinion leaders among the project communities, to sensitize them on the effects of the project implementation program on their socio-economic activities. The issue of potential emergence of PAPs was highlighted and participants appraised on the mitigation measures available to the project by development RAP report. The process of undertaking the assessment of the PAPs and their assets were explained to create awareness and form the basis for subsequent discussions. Public consultations were held at various selected locations as determined by administrative boundaries for ease of administration and management of the stakeholder's composition and record keeping for future reference and analysis. The list of public officials and civil society groups to be involved in the process was compiled to represent affected communities' interest on project development matters intervention. Moreover, most of the fora incorporated special arrangements to rope in civil society and vulnerable groups, youth, women and people with disability.

**Results of Cumulative Views, Concerns and Response During the Stakeholder Consultative Meetings:** In all the meetings, the PAPs and other stakeholders contributed freely on their perceptions on the roads project. They expressed positive and negative aspects of the project and sought the consultants' comments on their varied concerns of the project from pre-construction, construction and operation phases of the program implementation. After very thorough presentations by KE&T ESIA/RAP Consultant, GHA RAP Consultant and AfDB ESIA/RAP Consultant on the methodology for proposed project roads execution, intensive enquiries, contributions and questions were thrown up into the consultative process. The key issues from participants are summarized below in Table 15.0.

**Table 15.0 Profile of Key Issues, Comments and Responses During PAPs Consultative Meetings**

Serial Number	Name of PAP (Mobile number)	PAP Questions, Views, Issues, Comments	RAP Consultants Remarks, Response, Answers	Date of Meeting and Location
1	Abu Jerry (0277484187)	PAP wants to complete an uncompleted structure in RoW, should he go ahead and complete the structure?	PAP should stop work and adhere to the cut-off date of 15/08/2015. The property will be evaluated as part of the RoW assets valuation. Approved RAP document will indicate offer option for compensation payment (cash or	20/04/2018 (Asutware Junction)

			in-kind, after discussion with PAP for agreed option).	
2	Tetteh Amos Korley (0245225684)	Affected asset is a building in RoW, will he be compensated?	Option for compensation will be offered, but the option comes with negative side effect (cash could be spent).	20/04/2018 (Asutuare Junction)
3	Sakpti Robert (0553234352)	Asset of 40 plots of land affected in the RoW, should he develop the land?	PAP should not develop the land and obey the cut-off date. Option for compensation will be offered.	20/04/2018 (Asutuare Junction)
4	Kwabena Klagbjor (0244182133)	Prefers cash compensation for affected asset in RoW.	Approved RAP document will indicate the PAP entitlement and amount of cash compensation due him.	20/04/2018 (Asutuare Junction)
5	Mary Asantewaa (0243959472)	When will the project take-off?	Project Implementation undergo phases of approval: Preparation Phase – examines all activities involved and discuss with government; Appraisal Phase – involve project documentation finalization (AfDB Board approval); Money for the project will be approved and released. There is a conditionality – PAP should be fully compensated.	20/04/2018 (Asutuare Junction)

6	Gyamah George (0243471220)	Building materials dealer, how long will it take to pay compensation?	Disturbance to business will result in assessment of income loss. The property will be valued and disturbance allowance will be part of the entitlement for compensation payment.	20/04/2018 (Asutuare Town)
7	Kwabla Emmanuel (0241464793)	Compensation request for crops and land in RoW.	Assets will be valued and compensation payment made, depending on compensation option preferred by PAP.	20/04/2018 (Asutuare Town)
8	Peter Konor (0557694495)	Compensation request for affected house in Row.	After property inventory, compensation will be paid.	20/04/2018 (Asutuare Town)
9	John Tetteh Wayo (0541828929)	Compensation request for crops in RoW.	Compensation payment for crops after RoW assets assessment.	20/04/2018 (Volivo)
10	Matse Tewiah Gaglusu (0578778948)	Monetary compensation request for crops in RoW.	Compensation payment for crops after RoW assets valuation.	20/04/2018 (Volivo)
11	Isaac K. Amanor (0242859995)	Building plot and farmland in Row.	All affected assets will be valued and compensation payment made.	20/04/2018 (Volivo)
12	Patience Gaglusu (0546265966)	Building plot without structure in RoW.	Affected property will be assessed and preferred compensation payment made.	20/04/2018 (Volivo)
13	Kofi Atteh	Request for assurance of compensation payment for all	All affected properties or assets will be assessed and captured in the	20/04/2018 (Volivo)

		affected assets in the RoW.	RAP document for compensation payment to be made.	
14	Rejoice Etse	Affected church building in RoW compensation request.	Compensation payment will be made after church building valuation and capture in the RAP report.	20/04/2018 (Volivo)
15	Peter Tetteh Kwao (0244206359)	Should cash crop cultivation be continued after the cut-off date?	Yes, but up until the time the farmer receives compensation payment. There after the farmer will be expected to abandon the farmland for project implementation.	20/04/2018 (Volivo)
16	Joyce Teye	GHA posted "STOP WORK ORDER" on her new building project about 2 to 3 years ago. What should she do?	GHA prevented the continuation of the building project after the cut-off date of 15/08/2015.	20/04/2018 (Volivo)
17	Ametepey Adablah	Land owner who has rented his land to another person to farm. Request for compensation for the land and farm.	Valuation of the affected assets (land and farm crops) will be undertaken. Compensation payments will be made for the land and the crops separately as captured in the RAP document.	20/04/2018 (Volivo)
18	Jacob Treba	Request to know the general procedures for compensation payment, assessment and valuation of	General procedures for compensation payment are as follows: survey of road corridor; establishment of	20/04/2018 (Aveyime)

		assets, amount to be paid and list of PAPs.	RoW alignment; mapping and numeration of all affected assets; consultation and socio-economic census or survey of PAPs; development of list of PAPs; valuation and monetization of all affected assets and properties; preparation of RAP document indicating, cut-off date, entitlements of PAPs – vulnerable groups, household heads, etc., compensation amounts to be paid to PAPs.	
19	Gladys Yaa Nokodzi	Request for the need to stop work or authorization to continue development of parcel of land which has been bought and is in the RoW?	Authorization hereby issued by GHA for continuous development of your parcel of land. A new bypass alignment has been established to avoid all RAP issues associated with Section 2 Project Road at the Aveyime tight and very constricting cultural heritage corridor.	20/04/2018 (Aveyime)
20	Reverend W. K. Glover Mensah	Request for cash compensation for	Cash compensation will	21/04/2018 (Dofor Adidome)

	(0244446778)	farm crops affected in the RoW.	be paid after valuation of the assets as captured in the RAP document.	
21	Makafui Avemegah (0559293262)	Location of farm in RoW with photos already taken. Should she continue farming.	Yes, until compensation payment is made. After that, all farming activities should stop for contractor to possess the site.	21/04/2018 (Dofor Adidome)
22	Agbodzi Lotsu	Land intended for investor in RoW. Request for cash compensation.	Requested cash compensation will be paid as stipulated in the RAP report.	21/04/2018 (Dofor Adidome)
23	Martin Adu Kwadwo	Building structure in RoW, will compensation be paid?	Yes, as disclosed in the approved RAP document based on selected compensation option PAP wants.	25/05/2018 (Asikuma Junction)
24	Amankwa Cynthia	Building affected by the project RoW alignment. Request for cash compensation.	Approved RAP document will disclose the exact amount of money due PAP for compensation payment.	25/05/2018 (Asikuma Junction)
25	Victor Nyansafo	Farm crops have been affected by the project RoW alignment. How will compensation be paid?	The affected farm crops will be assessed and captured in an approved RAP report indicating the selected compensation option PAP prefers to be paid.	25/05/2018 (Asikuma Junction)
26	Juliana Samege	Compensation should be paid to all PAPs whose crops and farms will be displaced by the project.	Project has developed a RAP report covering all assets to be affected by the program.	25/05/2018 (Asikuma Junction)



			Compensation payment options discussed with PAPs and disclosed in the RAP document will be adhered to.	
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Source: Project Document Records

**Cumulative Attendance List of Stakeholders at Consultative Meeting:** Participants were mostly drawn from the project communities, including chiefs, elders, opinion leaders, youth leaders, women, elderly, pastors. The consultative meetings were very interactive and as open as possible for all participants to voice their views and concerns with regards to the project implementation program. The breakdown of participants by total numbers and location is disclosed in Table 16.0. Sample cumulative attendance list of participants at the consultative meetings has been indicated in Table 17.0. Since the original cumulative attendance list was hand written, clarity and readability of the output is a challenge. Therefore, a type written sample has been indicated in this report for evidence.

**Table 16.0 Consultative Meeting Participants by Total Numbers and Location**

Serial Number	Location	Total Number of Participants	Comments
1	Asutuare Junction	74	Section 1 Project Road
2	Asutuare Town	39	Section 1 Project Road
3	Volivo	160	Section 2 Project Road
4	Aveyime	15	Section 2 Project Road
5	Dofor Adidome	101	Section 3 Project Road
6	Asikuma Junction	25	Section 3 Project Road

Source: Project Document Records

**Table 17.0 Sample Cumulative Attendance List of Participants at Consultative Meetings**

Serial Number	Name of Participant	Location	Comments
1	<b>Genevive Azumah</b>	<b>Asutuare Junction</b>	<b>Section 1 Project Road</b>
2	Janet Tawiah	Asutuare Junction	Section 1 Project Road
3	Albert Adjorkatse	Asutuare Junction	Section 1 Project Road
4	Robert Kofi	Asutuare Junction	Section 1 Project Road
5	Fusana Nuhu	Asutuare Junction	Section 1 Project Road
6	Tetteh Wisdom	Asutuare Junction	Section 1 Project Road
7	Ramatu Sambo	Asutuare Junction	Section 1 Project Road
8	Juliana Narley	Asutuare Junction	Section 1 Project Road
9	Ofori Enoch Agu	Asutuare Junction	Section 1 Project Road
10	Evans Tetteh Kwao	Asutuare Junction	Section 1 Project Road
11	<b>Jonathan Kwabla</b>	<b>Asutuare Town</b>	<b>Section 2 Project Road</b>
12	Mary Akonor	Asutuare Town	Section 2 Project Road
13	Kwabla Emmanuel	Asutuare Town	Section 2 Project Road
14	Siedu Elizabeth	Asutuare Town	Section 2 Project Road
15	Leticia Kwasi	Asutuare Town	Section 2 Project Road
16	Sarah Adimiah	Asutuare Town	Section 2 Project Road

17	Philip Amanor	Asutuare Town	Section 2 Project Road
18	Konor Peter	Asutuare Town	Section 2 Project Road
19	Tetteh Nobi Samuel	Asutuare Town	Section 2 Project Road
20	Joseph Kwasi Awuku	Asutuare Town	Section 2 Project Road
<b>21</b>	<b>Hon. Philip Ayithey</b>	<b>Volivo</b>	<b>Section 2 Project Road</b>
22	Doe Martha	Volivo	Section 2 Project Road
23	Lucy Agbeko	Volivo	Section 2 Project Road
24	Alfred Dede	Volivo	Section 2 Project Road
25	Nartey Wayo	Volivo	Section 2 Project Road
26	Patience Teye	Volivo	Section 2 Project Road
27	Joseph Osei	Volivo	Section 2 Project Road
28	Akpoka Tettey	Volivo	Section 2 Project Road
29	Nene Ogbe Asumeh III	Volivo	Section 2 Project Road
30	Gakpo Emmanuel	Volivo	Section 2 Project Road
<b>31</b>	<b>Hon. Samuel Nyamede</b>	<b>Aveyime</b>	<b>Section 2 Project Road</b>
32	Kofi Azanu John	Aveyime	Section 2 Project Road
33	Yaa Norgbodzi	Aveyime	Section 2 Project Road
34	Agnes Norgbodzi	Aveyime	Section 2 Project Road
35	Wander Norgbodzi	Aveyime	Section 2 Project Road
36	David Treba	Aveyime	Section 2 Project Road
37	Francis treba	Aveyime	Section 2 Project Road
38	Freeman Torxe	Aveyime	Section 2 Project Road
39	Jacob Treba	Aveyime	Section 2 Project Road
40	Akpalu Mike	Aveyime	Section 2 Project Road
<b>41</b>	<b>Ho. Ben Avemegah</b>	<b>Dofor Adidome</b>	<b>Section 3 Project Road</b>
42	Hon. Charles Hodogbe	Dofor Adidome	Section 3 Project Road
43	Pastor Glover	Dofor Adidome	Section 3 Project Road
44	Asafoatse Dabah	Dofor Adidome	Section 3 Project Road
45	James Kabutey	Dofor Adidome	Section 3 Project Road
46	Avornu Charity	Dofor Adidome	Section 3 Project Road
47	Vivian Apotsi	Dofor Adidome	Section 3 Project Road
48	Celestina Dzatey	Dofor Adidome	Section 3 Project Road
49	Selina Akpabli	Dofor Adidome	Section 3 Project Road
50	Mercy Mansa Sraha	Dofor Adidome	Section 3 Project Road
<b>51</b>	<b>Kwasi Larbi</b>	<b>Asikuma Junction</b>	<b>Section 3 Project Road</b>
52	Veronica Rosemary Adams	Asikuma Junction	Section 3 Project Road
53	Amankwa Cynthia	Asikuma Junction	Section 3 Project Road
54	Juliana Samege	Asikuma Junction	Section 3 Project Road
55	Martin Worgbeie Dellai	Asikuma Junction	Section 3 Project Road
56	Cecilia Asare	Asikuma Junction	Section 3 Project Road
57	Juliana Obese	Asikuma Junction	Section 3 Project Road
58	Martin Adu Kwadwo	Asikuma Junction	Section 3 Project Road
59	Victor Nyansafo	Asikuma Junction	Section 3 Project Road
60	Kofi servor	Asikuma Junction	Section 3 Project Road

**Evidence of Grievance Redress Mechanism Presentation and Discussion with PAPs:** During the public consultations, the project communities were advised to form **Ad-hoc Project Grievance Redress Committees** to acquaint themselves with the impacts of the project and be able to articulate their views to optimize anticipated benefits of the project implementation program. They were informed that the project was dependent on their acceptance and their being able to effectively participate at every stage of the project development. They were informed that some solutions to grievances, concerns and complaints to drive the project were within their control particularly on matters touching on culture and traditions that can impact negatively on the project. Some of the issues that can cause delays and lead to dispute resolution mechanism application include:

- Clerical errors in data entry that leads to delays in processing of compensation for the PAPs.
- Emerging issues such as change in estate administration of affected properties causing delays to payment of compensation.
- Disputed ownership of an affected asset particularly where documentation is not reliable.
- Rejection of a compensation amount considered not adequate though representative of market value.
- Change of mind on compensation option already discussed and accepted by PAP, for example demanding land-for-land where cash payment had been preferred initially.
- Handling of cultural issues where there are no clear agreed precedents such as relocation of graves or payment of compensation in a polygamous marriage.

The main function of the **Grievance Redress Committee (GRC)** is to provide a forum for the PAPs to air their dissatisfaction arising from the compensation or implementation processes of the project. It is expected to fast-track addressing of emerging issues in a project that can derail the smooth implementation of that project. The committee is to receive complaints from PAPs through the project office either verbally or in writing and such complaints are to be addressed to the satisfaction of the complainant. If the complaint cannot be addressed to the satisfaction of the complainant within the prescribed period, the complainant may have recourse to the Grievance Committee. The complainant reserves the right to seek redress from the Court of Law which is lengthy and costly in most cases, where the PAP is still dissatisfied. The Grievance Redress Committee will compile registers of all complaints received from PAPs at the project office, the required actions and the decisions arrived at.

Once a complaint is received at the complaints desk in the project office, it is registered and given a reference number for ease of following up. The office will evaluate the application and determine if the issue can be handled administratively or the GRC will meet over the matter. The Grievance Category, Type and Relevant Authority involvement is shown in Table 18.0.

**Table 18.0 Grievance Category, Type and Relevant Authority**

Grievance Category	Grievance Type	Example/Comment	Authority Involvement
Complaint on Access	Complaints regarding loss of access to or degradation of farmland.	Where claimants contend wrongful assessment of type, amounts, condition or date of planting (before/after cut-off date) of assets by the census report.	GHA, Contractor, District or Municipal Assemblies.

Asset Valuation	Complaints regarding valuation of assets	Where a claimant believes that their assets were undervalued relative to market price and costs of replacement (including transaction costs) – such as in the case of mature trees.	GHA or LVA
Asset ownership/access to communal ecosystem services (wood, pasture, water, etc.)	Complaints regarding ownership of assets	In cases of shared ownership (for example of traditional pastureland used by nomadic group) where there is no clear ownership.	GHA, District or Municipal Assemblies.
Concerns regarding employment.	Complaints regarding loss of employment.	Where enterprises are closed or transferred as a result of land acquisition. Complaints will likely be in relation to resettlement entitlements.	GHA
Complaints regarding construction operations	Complaints regarding nuisance or other impacts during construction works.	Damage to or theft of personal property, accidents, antisocial behavior by project workers.	GHA, Contractor.
Concerns regarding employment	Complaints regarding lack of employment opportunities.	Affected stakeholders or project community residents claiming poor information regarding project employment vacancies.	GHA, Contractor
Complaints regarding cultural property	Complaints regarding archaeology, patrimony or other cultural heritage issue.	Graves, shrines and sacred trees or grove or forests.	Ghana National Museum of Monuments
Complaints regarding amenities.	Complaints regarding new amenities provided by Project.	Disagreement regarding new location or design of new facility (sports ground, public	GHA, Contractor, District or Municipal Assemblies.

		borehole, well, standing water pipe, etc.)	
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**Source: Project Document Data**

**Highlights of Future Stakeholders Engagement or Consultation Plan:** The importance of promoting public participation (and not only consultation) is to make the Project open, transparent and robust. Communication among all stakeholders is the key to public participation. Stakeholders were engaged separately as groups or as individuals representing their organizations. A Stakeholder Engagement Action Plan has been adopted by GHA as disclosed in **Table 19.0**.

**Table 19.0 Stakeholder Engagement Action Plan (2019 - 2021)**

Target Group	Purpose	Tasks/Methods	Schedule or Frequency	Responsible Parties	Project Phase & Cost (GHc)
GHA workers, management	Provide project updates and obtain feedback and concerns	Attend meetings. Record outcomes applicable to project communities and report on follow up at next meeting.	Quarterly throughout construction and operation.	GHA	All phases (50,000.00)
Project Surrounding Communities	Raise awareness and prepare communities on potential emergency scenarios that could be caused by the project, could affect the communities and how to respond to such scenarios.	Use workshops/focus group meetings to develop and test plans. Document plans and use leaflets to disclose widely within the communities.	Test plans annually	GHA/Contractor	Construction, Operation (50,000.00)
Directly and Indirectly Affected Communities	Provide affected communities with information on stakeholder's engagement program. Keep affected communities updated on project activities.	Prepare and disclose survey brochure on stakeholder engagement program to surrounding communities. Prepare and distribute publications including Community Newsletter, Fact Sheets, leaflets, etc.	End 2020	GHA/Contractor	Construction, Operation (50,000.00)

Employees and Contractor	Ensure workers and visitors are aware of the cultural context and briefed on obligations toward communities.	Implement community relations and cross-cultural awareness training at all project sites/offices. Prohibiting anti-social and illegal behavior.	As required	GHA/Contractor	Construction, Operation (20,000.00)
National Publication. Non-Governmental Organization	Provide opportunities for interested national stakeholder information on the project.	Public notices, public meetings and targeted consultation with key groups. Regular project reporting and updated content on website.	As required	GHA/Contractor	All phases (75,000.00)
National and Regional Govt	Keep Govt regularly briefed and updated on project activities	Update and briefing sessions with relevant Ministers.	As required	GHA/Contractor	All phases (50,000.00)
International Stakeholders	Provide opportunities for interested international stakeholders to obtain key information on project.	Regular project updates and key documentation provided on websites.	As required	GHA/Contractor	All Phases (40,000.00)
Community Consultation	Implementation of GHA obligations with regards to consultation with identified stakeholders and affected communities.	Implement ongoing project information. Campaign specifically targeting information for surrounding and encroached communities through leaflets on key topics; local, regional and national media articles.	As required	GHA/Contractor	All phase (50,000.00)
Management Systems	Maintaining management system	Maintain GHA/Contractor offices and community relation staff.	As required	GHA/Contractor	All phases (50,000.00)

		Maintain database and records of consultation activities.			
Grievance Mechanism	Implementation of provisions of Grievance Mechanism	Educate surrounding community residents in use of Grievance Mechanism procedure.	As required	GHA/Contractor	All phases (30,000.00)

**Source: Project Data Records**

## 5 Integration with Host Communities

The nature of the resettlement process, which is linear, does not involve complete relocation of affected persons from one geographical to another. In other words, the project will not be moving people (PAPs) from the current locality to new areas. A few of the PAPs who will lose buildings/structures will resettle in the same areas and if they will need to purchase plots elsewhere within the project zone, this will have no significance to the existing communities. Therefore, the issue of host communities did not arise.

## 6 Socio-Economic Studies

A census enumeration and socio-economic studies were conducted among the affected project communities. The major objectives of the surveys conducted on the sections 1,3 and 2 of the project roads include the following:

- To understand the scale and nature of the project impacts on project communities;
- To identify PAPs and map out their social and economic characteristics;
- To establish institutional arrangements for the implementation of the RAP activities;
- To obtain information needed for entitlement and compensation payments for lost assets; and
- To generate baseline data for monitoring and evaluation of livelihoods and income restoration and other sustainable development components.

The summary of key findings of the socio-economic studies for the sections 1, 3 and 2 of the project roads have been indicated in Tables 20.0, 32.0 and 45.0.

### 6.1 Socio-Economic Survey Studies Findings and Results for Asutuare Jn – Volivo Road (Section 1)

A total of 679 questionnaires were recorded out of 720 administered to the PAPs within the project direct impact zone. Categories of PAPs recorded as unknown and institution/public assets holders were 13 and 15 respectively. See Table 20.0.

**Table 20.0 Profile of PAPs on Asutuare Junction – Volivo Road (Section 1)**

Category of PAPs	Land	Buildings/Structures	Crops/Farms	Total Number of PAPs
Male	186	73	178	437



Female	105	39	98	<b>242</b>
Sub-Total	291	112	276	<b>679</b>
Unknown	2	9	2	<b>13</b>
Institution/Public Assets	2	13	-	<b>15</b>
<b>Total</b>	<b>295</b>	<b>134</b>	<b>278</b>	<b>707</b>

Source; GHA Data Records

**Sex Distribution of PAPs on Section 1 Road Project:** In all, 64% of the respondents were men, while 36% were women. More men responded, because of their higher level of visibility within the project corridor. Table 21.0 shows the sex distribution of respondents.

**Table 21.0 Sex Distribution of PAPs on Section 1 Project Road**

<b>Sex Type</b>	<b>Number of PAPs</b>	<b>(%)</b>
Male	437	64.3594
Female	242	35.6406
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Age Distribution of PAPs on Section 1 Road Project:** The age structure of majority of the respondents fell between 26 and 35 years (33%). The next age groups recorded were those within the ranges 18 to 25 years (27%); 36 to 45 years (14%); below 18 years (13%); 46 to 55 years (10%) and 56 to 65 years (2%) respectively. More noticeable was the presence of a comparatively small number of respondents over 66 years (1%). Table 22.0 shows the age distribution of respondents.

**Table 22.0 Age Distribution of PAPs on Section 1 Project Road**

<b>Age Range (years)</b>	<b>Number of PAPs</b>	<b>(%)</b>
Below 18	86	12.6657
18 – 25	185	27.2459
26 – 35	227	33.4315
36 – 45	92	13.5493
46 – 55	67	9.8675
56 – 65	12	1.7673
Over 66	10	1.4728
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Marital Status Distribution of PAPs on Section 1 Project Road:** Most of the respondents were married (67%). While a few of them, were single (23%), only small numbers were widowed (6%), divorced (2%) and separated (1%). The striking feature of most of the respondents being married is probably due to their movement into the project zone to engage in farming, trading and other economic activities. Table 23.0 shows marital distribution of PAPs on section 1 project road.

**Table 23.0 Marital Status Distribution of PAPs on Section 1 Project Road**

<b>Marital Status</b>	<b>Number of PAPs</b>	<b>(%)</b>
Married	457	67.3049
Single	159	23.4168
Widowed	40	5.8910
Divorced	14	2.0619

Separated	9	1.3255
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Educational Level Distribution of PAPs on Section 1 Project Road:** A relatively higher number of those surveyed have attained some formal level of education. About 35% have been educated to the level of tertiary school. The next formal level of education for the respondents is the middle school (28%). Then comes the next levels in the following order: JHS (13%); SHS (10%); no formal education (7%) and primary (6%). Table 24.0 shows educational level distribution of PAPs on section 1 project road.

**Table 24.0 Education Level Distribution of PAPs on Section 1 Project Road**

Education Level	Number of PAPs	(%)
Primary	42	6.1856
Middle	190	27.9823
Tertiary	238	35.0515
JHS	90	13.2548
SHS	69	10.1620
None	50	7.3638
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Profession/Occupation Distribution of PAPs on Section 1 Project Road:** An examination of the survey data shows some interesting patterns. Farming is the most predominant activity for the project communities. About 31% of all respondents are farmers, followed by traders (19%), others (15%), carpenters (14%) seamstresses (7%), drivers and hairdressers (5%) and businessmen (4%) respectively. See Table 25.0.

**Table 25.0 Profession/Occupation Distribution of PAPs on Section 1 Project Road**

Profession/Occupation	Number of PAPs	(%)
Farmers	211	31.0751
Traders	126	18.5567
Businessmen	29	4.2710
Drivers	37	5.4492
Seamstresses	45	6.6274
Hairdressers	34	5.0074
Carpenters	97	14.2857
Others	100	14.7275
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Income Distribution of PAPs on Section 1 Project Road:** The survey portrays that a little under half of the respondents (48%), earn more than GHc 500.00 per month. This is followed by 15%, 13%, 11% and 7% who earn in the range of GHc (401.00 and 500.00), GHc (301.00 and 400.00), GHc (100.00 and 200.00), GHc (201.00 and 300.00) and less than GHc100.00 per month respectively. See Table 26.0.

**Table 26.0 Income Distribution of PAPs on Section 1 Project Road**

Total Monthly Income (GHc)	Number of PAPs	(%)
Less than 100.00	45	6.6774

100.00 – 200.00	74	10.8984
201.00 – 300.00	50	7.3638
301.00 – 400.00	87	12.8130
401.00 – 500.00	100	14.7275
Over 500.00	323	47.5700
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Ethnic Group Distribution of PAPs on Section 1 Project Road:** Most of the respondents are part of a larger group called Ga Adangbes (75%). As a dominant ethnic group, the Ga Adangbes are the populous in the project zone of influence. They are followed by Ewes (11%), Gas (7%), others (6%) and Akans (2%). See Table 27.0.

**Table 27.0 Ethnic Group Distribution of PAPs on Section 1 Project Road**

<b>Ethnic Group</b>	<b>Number of PAPs</b>	<b>(%)</b>
Ga Adangbe	507	74.6686
Ewe	72	10.6038
Ga	45	6.6274
Akan	15	2.2091
Others	40	5.8910
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Religion Practice Distribution of PAPs on Section 1 Project Road:** Majority of the respondents are basically Christians (86%). However, the Muslims are in the minority as opposed to the Christians with 9% religion practice level. Traditional practice is about 4%, while the other 0.7% is distributed over atheists. But religious co-existence and tolerance levels are high and mutually inter-reactive and integrative. See Table 28.0.

**Table 28.0 Religion Practice Distribution of PAPs on Section 1 Project Road**

<b>Religion Type</b>	<b>Number of PAPs</b>	<b>(%)</b>
Christian	587	86.4507
Muslim	60	8.8365
Traditional	27	3.9764
Others	5	0.7364
<b>Total</b>	<b>679</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Size Distribution on Section 1 Project Road:** The data indicate that most of the households are single person households. Roughly equal proportions of households are composed of between 2 and 4 persons. This is followed by households with 5, 6 and 7 persons. See Table 29.0.

**Table 29.0 Household Size on Section 1 Project Road**

<b>Household Size</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>
1	15	20.5479	8	20.5128
2	10	13.6986	5	12.8205
3	10	13.6986	5	12.8205
4	10	13.6986	5	12.8205

5	9	12.3288	5	12.8205
6	7	9.5890	4	10.2564
7	4	5.4795	2	5.1282
8	3	4.1096	2	5.1282
9	2	2.7397	1	2.5641
10 (or more)	3	4.1096	2	5.1282
<b>Total</b>	<b>73</b>	<b>100.0</b>	<b>39</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Population by Composition and Sex on Section 1 Project Road:** The household normally consist of a head, with or without spouse, children, in-laws, parents, grandchildren and other relatives. The data shows that a total of 538 members live in households and that there are four (4) major components of the household composition, namely household heads (24.5%), spouses (10.2%) and children (39.2%). Grandchildren (9.8%) and other relatives (8.2%) constitute a significant proportion of the household composition. For the sexes, there are more males than females who are household heads 30.5% and 16.9% respectively. On the other hand, there are more females (11.4%) than males (9.3%) who are spouses. This pattern of household composition supports the view that the traditional household structure is very much a part of the wider social structure of the project zone. The proportion of non-relatives, parent/parent-in-law, step children and adopted/foster children are substantially low (1.9%, 1.1%, 0.7% and 0.4%). See Table 30.0.

**Table 30.0 Household Population by Composition and Sex on Section 1 Project Road**

<b>Relationship to Head</b>	<b>Total (Number)</b>	<b>Male (Number)</b>	<b>Female (Number)</b>	<b>Total (%)</b>	<b>Male (%)</b>	<b>Female (%)</b>
Head	132	92	40	24.5353	30.4636	16.9492
Spouse (wife/husband)	55	28	27	10.2230	9.2715	11.4407
Child (son/daughter)	211	106	105	39.2193	35.0993	44.4915
Parent/parent in-law	6	4	2	1.1152	1.3245	0.8475
Son/Daughter in-law	3	3	2	0.5576	0.6623	0.4237
Grand child	53	27	26	9.8513	8.9404	11.0169
Brother/Sister	18	10	8	3.3457	3.3113	3.3898
Step child	4	3	1	0.7435	0.9934	0.4237
Adopted/Foster child	2	1	1	0.3717	0.3311	0.4237
Other relative	44	23	21	8.1784	7.6159	8.8983
Non-relative	10	6	4	1.8587	1.9868	1.6949
<b>Total</b>	<b>538</b>	<b>302</b>	<b>236</b>	<b>100.0</b>	<b>100.0</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Population by Structure and Sex on Section 1 Project Road:** Household structure refers to the type of relationship (whether related or unrelated) among household members. The data shows that households composed of head, his or her spouse and children constitute the highest proportion of households in the project zone (26.1%), followed by households made up of head, spouse, children and

relative of head (22.1%) and single parent nuclear household (10.4%). Single parent extended and non-relative households constitute the lowest proportion of household (1.3%). See Table 31.0.

**Table 31.0 Household Population by Structure and Sex on Section 1 Project Road**

Household Structure	Total (Number)	Total (%)	Male (Number)	Male (%)	Female (Number)	Female (%)
Head only	27	5.0	14	5.6452	13	4.4828
Head and a spouse	9	1.7	5	2.0161	4	1.3793
Nuclear (Head, Spouse(s) & Children)	140	26.1	53	21.3710	87	30.0000
Extend (Head, spouse, children and Head relations)	119	22.1	59	23.7903	60	20.6897
Extended + non-relatives	9	1.6	4	1.6129	5	1.7241
Head, spouse (s) and other composition	22	4.0	10	4.0323	12	4.1379
Single parent Nuclear	56	10.4	27	10.8871	29	10.0000
Single Parent Extended	93	17.2	46	18.5484	47	16.2069
Single Parent Extended + non-relative	25	1.3	12	4.8387	13	4.4828
Head and other composition but no spouse	38	10.7	18	7.2581	20	6.8966
<b>Total</b>	<b>538</b>	<b>100.0</b>	<b>248</b>	<b>100.0</b>	<b>290</b>	<b>100.0</b>

Source: KE&T Data Records

## 6.2 Socio-Economic Survey Studies Findings and Results for Duffor-Adidome – Asikuma Junction Road (Section 3)

A total of 301 questionnaires were recorded out of 350 administered to the PAPs within the project direct impact zone. Categories of PAPs recorded as unknown and institution/public assets holders were 7 and 6 respectively. See Table 32.0.

**Table 32.0 Profile of PAPs on Duffor-Adidome – Asikuma Junction Road (Section 3)**

Category of PAPs	Land	Buildings/Structures	Crops/Farms	Total Number of PAPs
Male	83	41	80	<b>204</b>
Female	47	12	38	<b>97</b>
Sub-Total	130	53	118	<b>301</b>

Unknown	2	4	1	7
Institution/Public Assets	2	3	1	6
<b>Total</b>	<b>134</b>	<b>60</b>	<b>120</b>	<b>314</b>

**Source; GHA Data Records**

**Sex Distribution of PAPs on Section 3 Road Project:** About 68% of the respondents were men, while 32% were women. More men responded than women, because of their higher level of visibility within the project corridor. Table 33.0 shows the sex distribution of respondents.

**Table 33.0 Sex Distribution of PAPs on Section 3 Project Road**

<b>Sex Type</b>	<b>Number of PAPs</b>	<b>(%)</b>
Male	204	67.77
Female	97	32.23
<b>Total</b>	<b>301</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Age Distribution of PAPs on Section 3 Road Project:** The age pattern of majority of the respondents fell between 26 and 35 years (24%). The next age groups recorded were those within the ranges 18 to 25 years (23%); 36 to 45 years (19%); below 18 years (14%); 46 to 55 years (14%) and 56 to 65 years (4%) respectively. More noticeable was the presence of a comparatively small number of respondents over 66 years (3%). Table 34.0 shows the age distribution of respondents.

**Table 34.0 Age Distribution of PAPs on Section 3 Project Road**

<b>Age Range (years)</b>	<b>Number of PAPs</b>	<b>(%)</b>
Below 18	43	14.2857
18 – 25	68	22.5914
26 – 35	71	23.5880
36 – 45	57	18.9369
46 – 55	42	13.9535
56 – 65	12	3.9867
Over 66	8	2.6578
<b>Total</b>	<b>301</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Marital Status Distribution of PAPs on Section 3 Project Road:** Most of the respondents were married (45%). While a few of them, were single (35%), only small numbers were widowed (10%), divorced (7%) and separated (3%). The striking feature of most of the respondents being married is probably due to their movement into the project zone to engage in farming, trading and other economic activities. Table 35.0 shows marital distribution of PAPs on section 3 project road.

**Table 35.0 Marital Status Distribution of PAPs on Section 3 Project Road**

<b>Marital Status</b>	<b>Number of PAPs</b>	<b>(%)</b>
Married	135	44.8505
Single	106	35.2159
Widowed	30	9.9668
Divorced	20	6.6445
Separated	10	3.3223
<b>Total</b>	<b>301</b>	<b>100.0</b>

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**Source: KE&T Data Records**

**Educational Level Distribution of PAPs on Section 3 Project Road:** A relatively higher number of those surveyed have attained some formal level of education. About 35% have been educated to the level of middle school. The next formal level of education for the respondents is the primary school (21%). Then comes the next levels in the following order: Tertiary (13%); JHS (12%); no formal education (11%) and SHS (8%). Table 36.0 shows educational level distribution of PAPs on section 3 project road.

**Table 36.0 Education Level Distribution of PAPs on Section 3 Project Road**

<b>Education Level</b>	<b>Number of PAPs</b>	<b>(%)</b>
Primary	63	20.9302
Middle	104	34.5515
Tertiary	40	13.2890
JHS	37	12.2924
SHS	23	7.6412
None	34	11.2957
<b>Total</b>	<b>301</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Profession/Occupation Distribution of PAPs on Section 3 Project Road:** An examination of the survey data shows some interesting patterns. Farming is the most predominant activity for the project communities. About 35% of all respondents are farmers, followed by traders (32%), seamstresses (8%), businessmen (6%), carpenters (6%), drivers (5%) and others (3%) respectively. See Table 37.0.

**Table 37.0 Profession/Occupation Distribution of PAPs on Section 3 Project Road**

<b>Profession/Occupation</b>	<b>Number of PAPs</b>	<b>(%)</b>
Farmers	105	34.8837
Traders	96	31.8937
Businessmen	19	6.3123
Drivers	15	4.9834
Seamstresses	25	8.3056
Hairdressers	13	4.3189
Carpenters	18	5.9801
Others	10	3.3223
<b>Total</b>	<b>301</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Income Distribution of PAPs on Section 3 Project Road:** Most of the respondents (34%), earn less than GHc 100.00 per month. This is followed by 22%, 13%, 13%, 11% and 7% who earn in the range of GHc (100.00 and 200.00), GHc (401.00 and 500.00), GHc (201.00 and 300.00), over GH 500.00 and GHc (301.00 and 400.00) per month respectively. See Table 38.0.

**Table 38.0 Income Distribution of PAPs on Section 3 Project Road**

<b>Total Monthly Income (GHc)</b>	<b>Number of PAPs</b>	<b>(%)</b>
Less than 100.00	102	33.8870
100.00 – 200.00	66	21.9269
201.00 – 300.00	38	12.6246
301.00 – 400.00	22	7.3090

401.00 – 500.00	40	13.2890
Over 500.00	33	10.9635
<b>Total</b>	<b>301</b>	<b>100.0</b>

Source: KE&T Data Records

**Ethnic Group Distribution of PAPs on Section 3 Project Road:** Most of the respondents are part of a larger group called Ewes (70%). As a dominant ethnic group, the Ewes are the populous in the project zone of influence. They are followed by Ga Adangbes (19%), others (5%), Gas (4%) and Akans (3%). See Table 39.0.

**Table 39.0 Ethnic Group Distribution of PAPs on Section 3 Project Road**

<b>Ethnic Group</b>	<b>Number of PAPs</b>	<b>(%)</b>
Ga Adangbe	56	18.6047
Ewe	210	69.7674
Ga	12	3.9867
Akan	8	2.6578
Others	15	4.9834
<b>Total</b>	<b>301</b>	<b>100.0</b>

Source: KE&T Data Records

**Religion Practice Distribution of PAPs on Section 3 Project Road:** Majority of the respondents are basically Christians (66%). However, the Muslims are in the minority as opposed to the Christians with 15% religion practice level. Other practice is about 13% and is distributed over atheists, while the traditional is 5%. But religious co-existence and tolerance levels are high and mutually inter-reactive and integrative. See Table 40.0.

**Table 40.0 Religion Practice Distribution of PAPs on Section 3 Project Road**

<b>Religion Type</b>	<b>Number of PAPs</b>	<b>(%)</b>
Christian	199	66.1130
Muslim	46	15.2824
Traditional	16	5.3156
Others	40	13.2890
<b>Total</b>	<b>301</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Size Distribution on Section 3 Project Road:** The data indicate that most of the households are single person households. Roughly equal proportions of households are composed of between 2 and 4 persons. This is followed by households with 5, 6, 7 and 8 persons. See Table 41.0.

**Table 41.0 Household Size on Section 3 Project Road**

<b>Household Size</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>
1	8	19.5122	2	16.6667
2	6	14.6341	2	16.6667
3	6	14.6341	2	16.6667
4	6	14.6341	2	16.6667
5	5	12.1951	1	8.3333
6	4	9.7561	1	8.3333
7	2	4.8780	1	8.3333
8	2	4.8780	0	0



9	1	2.4390	0	0
10 (or more)	1	2.4390	1	8.3333
<b>Total</b>	<b>41</b>	<b>100.0</b>	<b>12</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Population by Composition and Sex on Section 3 Project Road:** The household normally consist of a head, with or without spouse, children, in-laws, parents, grandchildren and other relatives. The data shows that a total of 254 members live in households and that there are four (4) major components of the household composition, namely household heads (24.4%), spouses (10.2%) and children (39.4%). Grandchildren (9.8%) and other relatives (8.3%) constitute a significant proportion of the household composition. For the sexes, there are more males than females who are household heads 30.3% and 17.0% respectively. On the other hand, there are more females (11.6%) than males (9.2%) who are spouses. This pattern of household composition supports the view that the traditional household structure is very much a part of the wider social structure of the project zone. The proportion of non-relatives, parent/parent-in-law, step children and adopted/foster children are substantially low (1.2%, 1.2%, 0.8% and 0.8%). See Table 42.0.

**Table 42.0 Household Population by Composition and Sex on Section 3 Project Road**

Relationship to Head	Total (Number)	Male (Number)	Female (Number)	Total (%)	Male (%)	Female (%)
Head	62	43	19	24.4094	30.2817	16.9643
Spouse (wife/husband)	26	13	13	10.2362	9.1549	11.6071
Child (son/daughter)	100	50	50	39.3701	35.2113	44.6429
Parent/parent in-law	3	2	1	1.1811	1.4085	0.8929
Son/Daughter in-law	2	1	1	0.7874	0.7043	0.8929
Grand child	25	13	12	9.8425	9.1549	10.7143
Brother/Sister	8	5	3	3.1496	3.5211	2.6786
Step child	2	1	1	0.7874	0.7043	0.8929
Adopted/Foster child	2	1	1	0.7874	0.7043	0.8929
Other relative	21	11	10	8.2677	7.7465	8.9286
Non-relative	3	2	1	1.1811	1.4085	0.8929
<b>Total</b>	<b>254</b>	<b>142</b>	<b>112</b>	<b>100.0</b>	<b>100.0</b>	<b>100.0</b>

Source: KE&T Data Records

**Household Population by Structure and Sex on Section 3 Project Road:** Household structure refers to the type of relationship (whether related or unrelated) among household members. The data shows that households composed of head, his or her spouse and children constitute the highest proportion of households in the project zone (26.0%), followed by households made up of head, spouse, children and relative of head (22.0%) and single parent nuclear household (10.2%). Single parent extended and non-relative households constitute the lowest proportion of household (1.2%). See Table 43.0.

**Table 43.0 Household Population by Structure and Sex on Section 3 Project Road**

Household Structure	Total (Number)	Total (%)	Male (Number)	Male (%)	Female (Number)	Female (%)
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Head only	13	5.1181	7	5.9322	6	4.4118
Head and a spouse	5	1.9685	2	1.6949	3	2.2059
Nuclear (Head, Spouse(s) & Children)	66	25.9843	30	25.4237	36	26.4706
Extend (Head, spouse, children and Head relations)	56	22.0472	27	22.8814	29	21.3235
Extended + non-relatives	4	1.5748	1	0.8475	3	2.2059
Head, spouse (s) and other composition	10	3.9370	4	3.3898	6	4.4118
Single parent Nuclear	26	10.2362	12	10.1695	14	10.2941
Single Parent Extended	44	17.3228	21	17.7966	23	16.9118
Single Parent Extended + non-relative	3	1.1811	1	0.8475	2	1.4706
Head and other composition but no spouse	27	10.6299	13	11.0169	14	10.2941
<b>Total</b>	<b>254</b>	<b>100.0</b>	<b>118</b>	<b>100.0</b>	<b>136</b>	<b>100.0</b>

Source: KE&T Data Records

### 6.3 Socio-Economic Survey Studies Findings and Results for Asutuaire – Aveyime Road (Section 2)

A total of 428 questionnaires were recorded out of 470 administered to the PAPs within the project direct impact zone. Categories of PAPs recorded as unknown and institution/public assets holders were 8 and 18 respectively. See Table 44.0.

**Table 44.0 Profile of PAPs on Asutuaire – Aveyime Road (Section 2)**

Category of PAPs	Land	Buildings/Structures	Crops/Farms	Total Number of PAPs
Male	25	127	116	268
Female	10	57	93	160
Sub-Total	35	184	209	428
Unknown	-	3	5	8
Institution/Public Assets	-	17	1	18
<b>Total</b>	<b>35</b>	<b>204</b>	<b>215</b>	<b>454</b>

Source: GHA Data Records

**Sex Distribution of PAPs on Section 2 Road Project:** About 63% of the respondents were men, while 37% were women. More men responded than women, because of their higher level of visibility within the project corridor. Table 45.0 shows the sex distribution of respondents.

**Table 45.0 Sex Distribution of PAPs on Section 2 Project Road**

Sex Type	Number of PAPs	(%)
Male	268	62.6168
Female	160	37.3832
<b>Total</b>	<b>428</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Age Distribution of PAPs on Section 2 Road Project:** The age pattern of majority of the respondents fell between 26 and 35 years (30%). The next age groups recorded were those within the ranges 18 to 25 years (25%); below 18 years (13%); 36 to 45 years (12%); 46 to 55 years (9%); 56 to 65 years (8%) respectively. More noticeable was the presence of a comparatively small number of respondents over 66 years (3%). Table 46.0 shows the age distribution of respondents.

**Table 46.0 Age Distribution of PAPs on Section 2 Project Road**

Age Range (years)	Number of PAPs	(%)
Below 18	55	12.8505
18 – 25	106	24.7664
26 – 35	127	29.6729
36 – 45	52	12.1495
46 – 55	40	9.3458
56 – 65	34	7.9439
Over 66	14	3.2710
<b>Total</b>	<b>428</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Marital Status Distribution of PAPs on Section 2 Project Road:** Most of the respondents were single (40%). While the married ones were 39%, only small numbers were divorced (9%), widowed (8%) and separated (3%). The striking feature of most of the respondents being single is probably due to their movement into the project zone to engage in farming, trading and other economic activities. Table 47.0 shows marital distribution of PAPs on section 2 project road.

**Table 47.0 Marital Status Distribution of PAPs on Section 2 Project Road**

Marital Status	Number of PAPs	(%)
Married	168	39.2524
Single	172	40.1869
Widowed	36	8.4112
Divorced	40	9.3458
Separated	12	2.8037
<b>Total</b>	<b>428</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Educational Level Distribution of PAPs on Section 2 Project Road:** A relatively higher number of those surveyed have attained some formal level of education. About 32% have been educated to the level of middle school. The next formal level of education for the respondents is the SHS school (23%). Then comes

the next levels in the following order: JHS (17%); primary (17%); no formal education (8%) and Tertiary (3%). Table 48.0 shows educational level distribution of PAPs on section 2 project road.

**Table 48.0 Education Level Distribution of PAPs on Section 2 Project Road**

Education Level	Number of PAPs	(%)
Primary	72	16.8224
Middle	138	32.2430
Tertiary	11	2.5701
JHS	74	17.2897
SHS	100	23.3645
None	33	7.7103
<b>Total</b>	<b>428</b>	<b>100.0</b>

Source: KE&T Data Records

**Profession/Occupation Distribution of PAPs on Section 2 Project Road:** An examination of the survey data shows some interesting patterns. Farming is the most predominant activity for the project communities. About 29% of all respondents are farmers, followed by traders (23%), drivers (21%), seamstresses (8%), others (6%), carpenters (5%), hairdressers and businessmen (4%) respectively. See Table 49.0.

**Table 49.0 Profession/Occupation Distribution of PAPs on Section 2 Project Road**

Profession/Occupation	Number of PAPs	(%)
Farmers	125	29.2056
Traders	100	23.3645
Businessmen	17	3.9720
Drivers	88	20.5607
Seamstresses	35	8.1776
Hairdressers	18	4.2056
Carpenters	20	4.6729
Others	25	5.8411
<b>Total</b>	<b>301</b>	<b>100.0</b>

Source: KE&T Data Records

**Income Distribution of PAPs on Section 2 Project Road:** Most of the respondents (28%), earn between GHc (301.00 and 400.00) per month. This is followed by 23%, 15%, 15%, 9% and 9% who earn in the range of GHc (201.00 and 300.00), less than GHc 100.00, GHc (100.00 and 200.00), over GH 500.00 and GHc (401.00 and 500.00) per month respectively. See Table 50.0.

**Table 50.0 Income Distribution of PAPs on Section 2 Project Road**

Total Monthly Income (GHc)	Number of PAPs	(%)
Less than 100.00	66	15.4206
100.00 – 200.00	64	14.9533
201.00 – 300.00	99	23.1308
301.00 – 400.00	120	28.0374
401.00 – 500.00	39	9.1121
Over 500.00	40	9.3458
<b>Total</b>	<b>428</b>	<b>100.0</b>

Source: KE&T Data Records

**Ethnic Group Distribution of PAPs on Section 2 Project Road:** Most of the respondents are part of a larger group called Ewes (70%). As a dominant ethnic group, the Ewes are the populous in the project zone of influence. They are followed by Ga Adangbes (15%), Gas (8%), others (4%) and Akans (2%). See Table 51.0.

**Table 51.0 Ethnic Group Distribution of PAPs on Section 2 Project Road**

<b>Ethnic Group</b>	<b>Number of PAPs</b>	<b>(%)</b>
Ga Adangbe	65	15.1869
Ewe	300	70.0935
Ga	35	8.1776
Akan	10	2.3364
Others	18	4.2056
<b>Total</b>	<b>428</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Religion Practice Distribution of PAPs on Section 2 Project Road:** Majority of the respondents are basically Christians (77%). However, the Muslims are in the minority as opposed to the Christians with 18% religion practice level. Traditional practice is about 3%, while the others is 3% and is distributed over atheists. But religious co-existence and tolerance levels are high and mutually inter-reactive and integrative. See Table 52.0.

**Table 52.0 Religion Practice Distribution of PAPs on Section 2 Project Road**

<b>Religion Type</b>	<b>Number of PAPs</b>	<b>(%)</b>
Christian	328	76.6355
Muslim	76	17.7570
Traditional	13	3.0374
Others	11	2.5701
<b>Total</b>	<b>428</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Household Size Distribution on Section 2 Project Road:** The data indicate that most of the households are single person households. Roughly equal proportions of households are composed of 3, 5, 2, 4 and 6 persons. This is followed by households with 7, 8 and 10 persons. See Table 53.0.

**Table 53.0 Household Size on Section 2 Project Road**

<b>Household Size</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>
1	26	20.4724	11	19.2982
2	17	13.3858	8	14.0351
3	18	14.1732	9	15.7895
4	15	11.8110	8	14.0351
5	18	14.1732	7	12.2807
6	12	9.4488	5	8.7719
7	8	6.2992	3	5.2632
8	5	3.9370	3	5.2632
9	3	2.3622	1	1.7544
10 (or more)	5	3.9370	2	3.5088
<b>Total</b>	<b>127</b>	<b>100.0</b>	<b>57</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Household Population by Composition and Sex on Section 2 Project Road:** The household normally consist of a head, with or without spouse, children, in-laws, parents, grandchildren and other relatives. The data shows that a total of 883 members live in households and that there are four (4) major components of the household composition, namely household heads (24.5%), spouses (10.2%) and children (39.4%). Grandchildren (9.9%) and other relatives (8.3%) constitute a significant proportion of the household composition. For the sexes, there are more males than females who are household heads 30.2% and 17.1% respectively. On the other hand, there are more females (11.6%) than males (9.1%) who are spouses. This pattern of household composition supports the view that the traditional household structure is very much a part of the wider social structure of the project zone. The proportion of non-relatives, parent/parent-in-law, step children and adopted/foster children are substantially low (1.1%, 1.1%, 0.8% and 0.8%). See Table 54.0.

**Table 54.0 Household Population by Composition and Sex on Section 2 Project Road**

Relationship to Head	Total (Number)	Male (Number)	Female (Number)	Total (%)	Male (%)	Female (%)
Head	216	150	66	24.4621	30.2419	17.0543
Spouse (wife/husband)	90	45	45	10.1925	9.0726	11.6279
Child (son/daughter)	348	174	174	39.4111	35.0806	44.9612
Parent/parent in-law	10	7	3	1.1325	1.4113	0.7752
Son/Daughter in-law	7	4	3	0.7928	0.8065	0.7752
Grand child	87	45	42	9.8528	9.0726	10.8527
Brother/Sister	28	18	10	3.1710	3.6290	2.5840
Step child	7	4	3	0.7928	0.8065	0.7752
Adopted/Foster child	7	4	3	0.7928	0.8065	0.7752
Other relative	73	38	35	8.2673	7.6613	9.0439
Non-relative	10	7	3	1.1325	1.4113	0.7752
<b>Total</b>	<b>883</b>	<b>496</b>	<b>387</b>	<b>100.0</b>	<b>100.0</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Household Population by Structure and Sex on Section 2 Project Road:** Household structure refers to the type of relationship (whether related or unrelated) among household members. The data shows that households composed of head, his or her spouse and children constitute the highest proportion of households in the project zone (26.0%), followed by households made up of head, spouse, children and relative of head (22.1%) and single parent nuclear household (10.4%). Single parent extended and non-relative households constitute the lowest proportion of household (1.4%). See Table 55.0.

**Table 55.0 Household Population by Structure and Sex on Section 2 Project Road**

Household Structure	Total (Number)	Total (%)	Male (Number)	Male (%)	Female (Number)	Female (%)
Head only	44	4.9830	24	5.8394	20	4.2373
Head and a spouse	15	1.6988	6	1.4599	9	1.9068
Nuclear (Head,	230	26.0476	105	25.5474	125	26.4831

Spouse(s) & Children)						
Extend (Head, spouse, children and Head relations)	195	22.0838	94	22.8710	101	21.3983
Extended + non-relatives	14	1.5855	4	0.9732	10	2.1186
Head, spouse (s) and other composition	35	3.9638	14	3.4063	21	4.4492
Single parent Nuclear	92	10.4190	42	10.2190	50	10.5932
Single Parent Extended	152	17.2140	73	17.7616	79	16.7373
Single Parent Extended + non-relative	12	1.3590	4	0.9732	8	1.6949
Head and other composition but no spouse	94	10.6455	45	10.9489	49	10.3814
<b>Total</b>	<b>883</b>	<b>100.0</b>	<b>411</b>	<b>100.0</b>	<b>472</b>	<b>100.0</b>

Source: KE&T Data Records

## 6.4 Vulnerability Analysis

As recommended by the AfDB involuntary resettlement policy, attention should be paid to the needs of disadvantaged groups among those displaced. Vulnerable individuals or groups can be defined as those within a project's area of influence who are particularly marginalized or disadvantaged and who might thus be more likely than others to experience adverse impacts from a project. Vulnerability can be determined by identifying the likelihood that an individual or a group faces harder conditions as the result of the implementation of a project. This vulnerable status may stem from a group's gender, economic status, ethnicity, religion, cultural behaviour, sexual orientation, language or physical and psychological health conditions. Vulnerable groups may include, among others, female-headed households, those below the poverty line, the landless, those without legal title to assets, ethnic, religious and linguistic minorities, Indigenous Peoples, those who are disabled, etc.

**Female Headed Household:** Headship for female headed households has been identified as one of the vulnerable groups. On this basis, the total number of PAPs considered vulnerable and will require additional support are the following: Section 1 Project Road (13 females); Section 3 Project Road (6 females) and Section 2 Project Road (20 females). See Table 56.0.

**Table 56.0 Vulnerable Female Headed Households**

Serial Number	Female (Frequency)	Female (%)	Project Road Section
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1	13	33.3333	1
2	6	15.3846	3
3	20	51.2821	2
<b>Total</b>	<b>39</b>	<b>100.0</b>	-

Source: KE&T Data Records

**PAPs Below Poverty Level:** Another group has been included as very vulnerable, namely PAPs below poverty level (less than USD 2.00 per day or less than GHc 100.00 per month). On this basis, the total number of PAPs considered very vulnerable and will require additional support are the following: Section 1 Project Road (42 males and 24 females); Section 3 Project Road (69 males and 33 females) and Section 2 Project Road (28 males and 17 females). See Tables 57.0.

**Table 57.0 Very Vulnerable PAPs Below Poverty Level**

Serial Number	Male (Frequency)	Male (%)	Female (Frequency)	Female (%)	Project Road Section
1	42	30.2158	24	32.4324	1
2	69	49.6403	33	44.5946	3
3	28	20.1439	17	22.9730	2
<b>Total</b>	<b>139</b>	<b>100.0</b>	<b>74</b>	<b>100.0</b>	-

Source: KE&T Data Records

## 7 Legal Framework, Including Mechanisms for Conflict Resolution and Appeal

**Introduction:** The FRAP is prepared in accordance with the Ghana Government Legislations and International Applicable Policies and Requirements (including AfDB Guidelines and Safeguard Policies) that govern preparation of Resettlement Action Plans. The relevant national legislations and international applicable policies and requirements considered among others are as follows:

### 7.1 Ghanaian Laws

- Legal Framework for Expropriation and Compensation.
- Constitution of the Republic of Ghana, 1992.
- State Land Act, Act 125 of 1962.
- Statutory Wayleave Instrument Act 186 1963.
- State Lands Act (1962) Act 125 (As Amended).
- Ghana Land Policy 1999.
- Interest in Land
  - Allodial Title
  - Customary Freehold
  - Customary Tenancies
  - Common Law Freehold
  - Leasehold
- Compensation Awards

#### 7.1.1 Legal Framework for Expropriation and Compensation



The Ghanaian Law provides that involuntary acquisition of private property must be done in accordance with laid down statutory procedures. In the areas of land administration one of the critical policies of the Government of Ghana is that fair and adequate compensation or alternative resettlement assistance is provided for eligible people who for the sake of national interest to surrender their interest in land or landed properties to the state for development.

As the roads project of GHA are land based, various interests and titles to pieces of land will be impacted. Therefore, an analysis of the legal framework for the roads project will be done in the RAP and this will consider the various land holding arrangements in the assessment of compensation for the various interest for lands taken. Some of these interests in land originate from Ghanaian customary law and tradition; and some are derived from English common law which has been assimilated into the Ghanaian Law.

### 7.1.2 Constitution of the Republic of Ghana

The 1992 Constitution guarantees private property rights. Interference with ownership and enjoyments of such rights is discouraged by the constitution. Even the country's sovereign and inherent powers of eminent domain over private property is equally controlled and can be exercised only when it is in the public purpose to do so. The 1992 Constitution stipulates in article 20 (1) that: "No property of any description of interest in or rights over any property shall be compulsorily taken possession of or acquired by the state unless the following conditions are satisfied: i) the taking of possession is necessary in the interest of defense, public mortality, public safety, public order, public health, town and country planning or development or utilization of a property in such manner as to promote the public benefit and ii) the necessity for the acquisition is clearly stated as to provide justification for causing any hardship that may result to any person who has an interest in or right over the property."

Further article 20 (2) provides that such compulsory acquisition shall only be made under a law which makes provision for: i) the prompt payment for fair and adequate compensation ii) right of access to the high court by any aggrieved person.

Article 20 (3) stipulates that where compulsory acquisition of land involves the displacement of any inhabitants, the state shall resettle them on suitable alternative lands with due regards to their socio-cultural values. Another key provision worthy of note is that, where any property compulsorily acquired is not used for the stated purpose or in a public interest the owners are to be given the first option of taking back their lands subject to the refund of compensation received or payment of commensurate amounts for the property.

### 7.1.3 State Land Act, Act 125 of 1962

This is an Act to provide for the acquisition of land in the national interest and or other purposes connected therewith. The relevant extracts to support this Act are section 1: item 1, 2 and 3, sections 2, section 4 as well as section 4A of the Amendment Decree of 1974.

### 7.1.4 Statutory Wayleave Instrument Act 186 1963

This Act is probably the most relevant Act in place with regards to land acquisition for roads project. Section 1 of this Act empowers the President whenever, in his opinion, it is in the public interest to i) provide for entry on any land for construction and maintenance of public interest ii) provide for entry on any land for construction and maintenance of public works and for the creation of RoW's and other similar rights with

respect to such works. Works are to be provided by the local authority or any statutory organization, corporation or department. If this means of compulsory acquisition is undertaken, an application is made to the Lands Commission, which the consults with the Minister of Lands and Forestry to continue with the transfer process.

The instrument for the acquisition must contain the following elements: i) description (with measurements) of the land affected by the Lands Act together with a plan showing the position of the works to be constructed thereon ii) particulars of the person or body receiving the benefits of the Executive Instrument iii) a copy of a wayleaves instrument shall be served on the owner or occupier of the land affected by the executive instrument and, if neither the owner nor occupier can be found, it shall be posted in a conspicuous place on the land and published in a local newspaper.

The right may be terminated in accordance with Lands (Act 186) Regulation 1963, including revocation by the President based on the recommendation of the Ministry of Lands and Forestry after the instrument ceases to be used for the purpose for which it was granted. Of the various pieces of legislation that provide the basis for compulsory acquisition of land, the Lands (Statutory Wayleaves) Act of 1963 is the most relevant for present purposes. The Lands (Statutory Wayleaves) Regulations as amended prescribe the procedure for acquisitions.

#### 7.1.5 State Lands Act (1962) Act 125 (As Amended)

Act 125 (as amended) is the main enactment normally used for compulsory acquisition. It provides that the President may by Executive Instrument (EI) acquire any land for the public interest. The modalities for the acquisition are spelt out in the Act and its Regulation-State Lands Regulation (1962) LI 230 and culminate in the publication of an EI. Once the EI is published, the lands specified are deemed acquired and vested in the President. Consequently, all previous interests are extinguished.

The Act provides for compensation payment based on market or replacement values. Lump sum compensation is prescribed under the Act. Cost of disturbance and other incidental expenses or damages are to be considered in the award of compensation. The major limitation of Act 125 is that provision has not been made for public consultation and involvement in the acquisition process. As effective consultation is crucial for the roads project implementation, the GHA will conduct extensive consultations with the affected locations and with other stakeholders at every stage of the roads project implementation.

#### 7.1.6 Interest in Land

There are five (5) main interests under laws and land use rights. These interests are: Allodial Tittle; Customary Freehold; Customary Tenancies; The Common Law Freehold and the Freehold.

##### 7.1.6.1 Allodial Title

In the Ghanaian context, this is the highest interest capable of being held in land. The Allodial tittle is customarily communally owned and is generally held or vested in stools or skins. In some traditional areas, it is held by clans, families or individuals. Being generally in the form of communal interest in land it accrues to the entire community and is administered by the recognized traditional authority. The owner of the allodial title has complete and absolute freedom to use and dispose of the land only subject to the restrictions, or limitations or obligations as may be imposed by the general laws of the country. The mode

of acquisition of the allodial title is by: discovery by hunters or pioneers of the stool etc. of unoccupied land and subsequent settlement thereof and use by the subject; conquest; purchase or gift.

#### 7.1.6.2 Customary Freehold

The customary freehold is an interest or title which a member of the larger community which holds the allodial title acquires in the communal land. It is an interest which is held as of right by virtue of being a member of the community. It is of indefinite duration and thus potentially subsists forever. The member who holds such interest has the right of beneficial occupation; unfettered use (also subject to the laws of the country). Upon death, the interest devolves on his/her successors in title and infinitum. This interest prevails against the whole world including the allodial title from which it was derived. The customary freehold may however be terminated by the occurrence of any of these occasions: (i) failure of successors; (ii) compulsory acquisition by the state iii) sale or gift by owner; (iv) abandonment or forfeiture in rare circumstances where for example holders denies the absolute title of the allodial owner.

#### 7.1.6.3 Customary Tenancies

These are lesser interest in land and are created by the holder of the allodial title or customary freehold (or common law freehold). These types of tenancies are in nature share cropping arrangements. They are quite common in Ghana and occur when a tenant-farmer gives a specified portion of the farm produce to the land owner at each harvest time in consideration for the use of the land. The two population tenancy arrangements are the “Abusa” and “Abunu” schemes. Other forms of customary tenancies in which the consideration from the tenant is not sharing of crops but cash or a combination of crops and money exist. The customary license is in this category.

#### 7.1.6.4 Common Law Freehold

This is an interest held for an indefinite period. It is derived from the rules of common law. The holder of this interest has the right of beneficial occupation and may subject to the laws of the land use in any manner. This type of freehold is created only by express grant. The grantor may thus impose terms on the grantee provided such terms are reasonable and not contrary to public policy or unconscionable. Currently, the laws of the land forbid non-Ghanaians from acquiring freehold in lands in Ghana.

#### 7.1.6.5 Leasehold

This type of interest is also a creation of the common law and not Ghanaian customary law. It is an interest in land for specified period. The leasehold may be granted by the allodial holder in respect of lands in which no conflicting interest exists, or by a customary freeholder, or common law freeholder. In Ghana, leasehold may be for a maximum duration of 99 years (again non-Ghanaians can only acquire leases up to 50 years). Various terms and conditions may be imposed by the grantor including the payment of rent as consideration for the grant.

#### 7.1.7 Compensation Awards

The laws on expropriation recognize the importance of compensation payments to make up for losses suffered. The 1992 constitution for instance frowns on the exercise of the eminent domain without prompt payment of adequate compensation. The basic government policy behind compensation awards is that no

one should be made worse off as a result of the implementation of any project. Accordingly, any person who establishes identifiable and legally recognized interest/rights is entitled to compensation. The laws provide that the compensation claims are to be lodged by the affected people within six months of acquisition. In most cases the claims are submitted by private professional valuers on behalf of claimants.

The LVD which is the government agency mandated to value lands and properties acquired by Government and advice on compensation to be paid carries out field inspection, referencing of affected properties and prepare the list of claimants affected and the losses suffered. In carrying out the assessment of compensation the following broad principles are followed:

- Market replacement prices are considered.
- Value of property/asset to the existing owner and not the acquiring agency.
- Values incorporate all losses and benefits flowing from the assets affected.
- No account is to be taken of any improvement unless the improvement was done in good faith and not in contemplation of the acquisition.

Application of the principles ensures that the compensation offered is fair and adequate. And upon completion of assessment, payment is immediately affected; where there are no conflicts to the claims submitted or disagreements in the amounts offered. Where compensation is accessed but cannot be paid owing to a dispute or conflicts, the government is required to lodge the amount in an interest yielding escrow account.

## 7.2 International Applicable Policies and Requirements

- AfDB Environmental Policy, 2004.
- AfDB Integrated Safeguard Standards, 2013 – Operational Safeguard (OS 2) on Involuntary Resettlement, including OS-1, OS-4 and OS-5.
- AfDB Gender Policy, 2001.
- Bank Group Policy on Poverty Reduction, 2004.
- Policy and Guidelines on Cooperation with Civil Society Organizations (CSOs), 1999.

The African Development Bank (AfDB) set out five (5) main policy statements and operational safeguards requirements that the Bank's clients are expected to meet when addressing social and environmental impacts and risks. These five operational safeguards (OS) are outlined as follows: (i) OS 1: Environmental and Social Assessment; (ii) OS 2: Involuntary Resettlement: Land Acquisition, Population Displacement and Compensation; (iii) OS 3: Biodiversity and Ecosystem Services; (iv) OS 4: Pollution Prevention and Control, Greenhouse Gases, Hazardous Materials and Resource Efficiency; (v) OS 5: Labor Conditions, Health and Safety.

The OS-2 consolidates the policy commitments and requirements set out in the Bank's policy on involuntary resettlement, and it incorporates refinements designed to improve the operational effectiveness of those requirements. It embraces comprehensive and forward-looking notions of livelihood and assets, accounting for their social, cultural, and economic dimensions. It also adopts a definition of community and common property that emphasizes the need to maintain social cohesion, community structures, and the social interlinkages that common property provides. The safeguard retains the requirement to provide compensation at full replacement cost; reiterates the importance of a resettlement that improves standards of living, income earning capacity, and overall means of livelihood; and emphasizes the need to ensure that

social considerations, such as gender, age, and stakes in the project outcome, do not disenfranchise project-affected people.

### 7.3 Comparison of Ghanaian Laws and AfDB Policies and Practices

Comparison of Ghanaian Laws and AfDB Policies and Practices demonstrate how they address gaps between domestic requirements and international standards and requirement. To overcome these issues, GHA will incorporate a comprehensive gap analysis together with proposed measures to address the gaps in the design of its RAP, thus ensuring full compliance with AfDB OS - 2. **Table 58.0** below displays both domestic and international requirements and where required states how any gaps identified are addressed.

**Table 58.0 Gap Analysis – Comparison of Ghanaian Laws and AfDB Policies and Practices**

Resettlement Topic	Ghanaian Legislative Requirements	AfDB Policies and Practices	Gap	Proposed Strategy
<b>Calculation of Compensation</b>	Fair and adequate. The Act requires that the amount of compensation, subject to approval of the Land Valuation Division, be determined by agreement between the parties concerned.	Compensation should be adequate to replace lost land and other assets at full replacement cost in local markets.	National Law requires approval of Govt valuer. AfDB OS -2 stress full market replacement costs.	Compensation rates are determined by Independent Valuer at market rates, approved by LVD and agreed through negotiations.
<b>Cash Compensation</b>	Constitution of Ghana protects Ghanaians from deprivation of property. Land can only be accessed once “prompt payment of fair and adequate compensation prior to the taking of possession of the property” has been undertaken.	<p>When payment of cash compensation is considered, the abilities of the affected population to utilize cash to restore standards of living should be carefully assessed.</p> <p>Staggered compensation payments may be made where one-off cash payments would demonstrably undermine social and/or resettlement objectives, or where there are ongoing impacts to livelihood activities.</p> <p>Where it is likely that cash compensation may be diverted from longer-term investment to short-term consumption, payment of in-kind compensation (e.g. land, livestock or other moveable/transferable property) may be more appropriate.</p>	The project proponents are not required by Ghanaian laws to consider compensation in-kind although the EIA Guidelines refer to the “constraints to monetary compensation.”	The project commits to compensating affected persons for loss of right over land or property in-kind in preference over cash.
<b>Census</b>	Under Ghana EPA procedures, economic and physical displacement impacts are addressed under socio-economic impacts and a Resettlement Action Plan or Compensation Plan. These documents must include a budget as proof that a census has been carried out to collect appropriate socio-economic baseline data to identify PAPs who will be displaced by the project and will be eligible for compensation and assistance.	Where involuntary resettlement is unavoidable, either as a result of a negotiated settlement or expropriation, a census will be carried out to collect appropriate socio-economic baseline data to identify the persons who will be displaced by the project, determine who will be eligible for compensation and assistance.	Ghana EPA Guidelines require that a census be conducted in the event of compensation and resettlement.	The project commits to identifying all persons who will be displaced as a result of the project implementation program. This will be done in the form of a census prior to resettlement to determine who will be eligible for compensation and assistance.

<b>Replacement Housing</b>	Displaced persons are to be resettled on suitable land with due regard for their economic well-being and social and cultural values.	If people in the project area must move, they will be: offered a choice of resettlement options including replacement housing or cash compensation and provided relocation assistance, with attention given to the poor and vulnerable.	AfDB OS-2 specifies that replacement properties must be equal or higher value and that there must be both a choice of properties, replacement in-kind being the preferred option and a cash alternative where appropriate.	Resettlement, Relocation and Compensation Policy Frameworks will be prepared to meet both national and AfDB OS - 2.
<b>Eligibility</b>	Under Ghanaian Laws, there are five (5) main interests and laws and land use rights. They are Alloidal Title, Customary freehold, Customary Tenancies, The Common Law Freehold and the freehold. Under these different types of land ownership, occupants and land owners are eligible for compensation for land, improvements to land, crops and structures.	Eligibility criteria should recognize the rights of those affected people: Who have formal legal rights to the land or assets they occupy or use Who do not have formal legal rights to land or assets, but have a claim to land that is recognized or recognizable under national law.  Who have no recognizable legal right or claim to the land or assets they occupy or use.  The census will establish the status of the displaced persons.	Ghanaian Laws and the AfDB OS - 2 are consistent in the recognition of the rights of occupants and land owners under the different land tenure regimes.	The project will recognize the rights of all affected people including those with formal legal rights, those without formal legal rights and those who have no recognizable legal right to land or assets used or occupied by them. The census to be undertaken prior to land acquisition and resettlement activities will establish the status of the displaced persons.
<b>Alternative Project designs</b>	Under Ghana EPA Guidelines development activities must be exercised in a manner that minimally affects land owners	Feasible alternative Project designs should be considered to avoid or at least limit physical or economic displacement.	AfDB OS - 2 explicitly require that resettlement be minimized by considering alternative project designs.	The project commits to avoiding or minimizing impacts by considering alternative project designs.
<b>Resettlement</b>	Displaced persons are to be resettled on suitable land with due regard for their economic well-being and social and cultural values.	Physically displaced persons will be offered a choice of replacement property of equal or higher value, security of tenure, equivalent or better characteristics, and advantages of location or cash compensation where appropriate. Compensation in kind should be considered in lieu of cash. Compensation levels will be at full replacement cost in local markets.	AfDB OS – 2 specifies that replacement properties must be equal or higher value and that there must be both a choice of properties, replacement in-kind being the preferred option and a cash alternative where appropriate.	Preparation of Resettlement, Relocation and Compensation Policy Framework to meet both national and AfDB OS - 2.
<b>Vulnerable Groups</b>	The Ghanaian Constitution of 1992 states that “ the State shall take affirmative action in favor of groups marginalized on the basis of gender, age, disability or any other reason for the purpose of addressing imbalances existing against them,” This regulation is not fully described in the context of	In resettlement planning, attention should be given to the poor and vulnerable. Compensation in kind should be offered to facilitate permanent relocation and establishment at an alternative location.	The Ghanaian Laws offer protection of vulnerable groups from exploitation. However, this does not provide the procedure for this category of PAPs to be consulted, engaged and further protected.	The Project commits to identifying vulnerable PAPs. Resettlement packages will give preference to compensation in-kind with special facilitation program to be developed with key stakeholders.

	resettlement and land acquisition. However, Ghana EPA Guidelines require that vulnerability is established during census.			
<b>Livelihood Restoration</b>	Ghanaian Legislation require development projects to resettle impacted people on suitable alternative land with the objective to improve livelihoods and standards of living.	<p>Economically displaced persons whose livelihoods or income levels are adversely affected will also be provided opportunities to improve, or at least restore, their means of income-earning capacity, production levels, and standards of living:</p> <p>Land based livelihoods – land with a combination of productive potential, locational advantages, and other factors at least equivalent to that being lost.</p> <p>If land or similar resources is unavailable - alternative income earning opportunities (e.g. credit facilities, training, cash, or employment opportunities). Cash compensation alone, however, is frequently insufficient to restore livelihoods.</p>	National Law does not specify assistance, but AfDB OS - 2 requires targeted assistance in terms of livelihood supports.	Resettlement assistance in terms of moving assistance and allowance forms part of compensation framework, in addition to loss of earnings assistance and longer-term livelihood development programs
<b>Economic Displacement</b>	Ghanaian Laws require that projects involving physical displacement cash compensation only. Economic displacement is not addressed exclusively if it is not in addition to a physical displacement. It is only addressed if it is a business that can prove their loss by providing an audited account.	Land acquisition for the project may result in the physical displacement of people as well as their economic displacement. As a result, requirements for both physical displacement and economic displacement may apply.	With physical displacement provide whatever non-monetary assistance and compensation is required. Informal businesses should also have their losses identified and addressed.	With physical displacement provision of whatever non-monetary assistance and compensation will be required. With economic displacement, provide compensation to enable PAPs to improve or restore their income-earning capacity.
<b>Consultation and Information Disclosure</b>	Consultation as part of ESIA procedures is mandatory. There is provision for consultation relating to involuntary resettlement in Ghanaian legislation with the establishment of Resettlement Monitoring Committee with community participation.	<p>Must ensure that resettlement activities apply the principle of free, prior and informed consultation.</p> <p>Ensure that women's perspectives and their interests are factored into all aspects of resettlement planning and implementation.</p> <p>Disclosure of relevant information and participation of PAPs will continue during the planning, implementation, monitoring, and evaluation of compensation payments, livelihood restoration activities, and resettlement.</p>	Ghanaian legislation does not define consultation procedures in detail as required by AfDB OS - 2 which requires active participation of those affected.	Consultation procedures throughout the project cycle shall be implemented in accordance with AfDB OS - 2. These shall cover affected persons host communities (if applicable). NGOs and community leaders will be involved as appropriate. Opportunities will be provided to affected persons to participate in the planning, implementation and monitoring of the resettlement program including compensation measures, additional



				livelihood restoration proposals and the timing of activities. Detailed Stakeholder Engagement Plan and Communications Strategy will be provided to ensure informed participation throughout project cycle (from inception to completion).
<b>Grievances</b>	Informal conflict resolution and formal judicial procedures are available. Disputes regarding compensation will be addressed. In addition, any person who has an interest or right over property being acquired compulsorily has a right of access to a court.	Must establish a grievance mechanism to receive and address specific concerns about compensation and relocation, including a recourse mechanism designed to resolve disputes in an impartial manner.	Ghanaian Laws do not explicitly require that development project implementers develop independent grievance redress mechanisms that are accessible to affected communities and individuals. However, Ghana EPA Guidelines make provision for a Grievance Committee formation. Moreover, AfDB OS - 2 requires specific grievance mechanism to be in place.	Detailed grievance mechanism will be put in place and articulated through Stakeholder Engagement Plan. Moreover, this plan will be made accessible to PAPs, including free access to judicial and administrative remedies.
<b>Monitoring and Evaluation</b>	No requirement under Ghanaian Law although due legal processes be observed. Under Ghana EPA Guidelines, A Resettlement Monitoring Committee must be established with community participation.	Requires monitoring and reporting on the effectiveness of RAP implementation.	A monitoring system to evaluate the implementation of the RAP and the impact of involuntary resettlement on PAPs is not required under Ghanaian Law. However, AfDB OS - 2 requires effective monitoring and evaluation of the effectiveness of RAP implementation.	Detailed monitoring and evaluation program will be built into the overall project management process. This system will include both an internal system to monitor the progress and implementation of the RAP and, if deemed necessary, an external independent process to monitor the short and medium terms impacts of involuntary resettlement on affected persons.

## 7.4 Grievance Redress Mechanism

Grievances are issues, concerns, problems or claims (to be perceived or actual) that an individual or a community group wants a project proponent to address or resolve. In order to deal with the grievances that may arise during the implementation of the FRAP, there is need to incorporate a grievance redress process within the FRAP. During the project implementation, the stakeholders (mostly the communities within the project enclave) could have some grievances with respect to the project activities, their impacts, compensation and other mitigation measures. Some of the key issues of these grievances could include:

- PAPs not listed.
- Losses (such as damaged crops) not identified correctly.
- Compensation inadequate or inappropriate.
- Dispute about ownership and entitlement.
- Delay in disbursement of compensation.
- Improper distribution of compensation in case of joint ownership.
- Misidentification of owner/occupier of eligible property and assets.
- Errors in asset assessment.
- Disputes over plots limits, either between the affected person and the project, or between two (2) neighbors.
- Complaints about entitlement policy of the project.
- Disagreement of asset valuation.
- Discontent over time and manner of compensation.
- Disputed ownership of businesses (for example if the owner and the operator are different persons).
- Any other issues arising during the project implementation.

In order to address the above issues, a Grievance Redressal Mechanism (GRM) will be devised. The main objective of the GRM will be to provide a mechanism to mediate conflict and cut down on lengthy litigation, which often delays infrastructure projects like this. It will also facilitate people who will have objections or concerns about their assistance, a public forum to raise their objections through conflict resolution, address these issues adequately. The main functions of the GRM will be as follows:

- Provide a mechanism to the PAPs on problems arising as a result of project activities.
- Record the grievance of the PAPs, categories and prioritize the grievances that need to be resolved.
- Report to the aggrieved parties about the developments regarding their grievances and the decision of the project authorities.

Under the GRM, a Social Complaint Register (SCR) will be maintained at the project sites to document all complaint received from the local communities. The information recorded in the Register will include the following:

- Date of the complaint.
- Background information of complainant
- Description of the grievance.
- Actions to be taken.

- Person responsible to take the action.
- Follow-up requirements.
- Target date for the implementation of the mitigation measure.

#### 7.4.1 Formation of Resettlement Monitoring Committee (RMC)

GHA will prepare terms of reference (TOR) for the RMC for the Project. The RMC representatives will be vetted by GHA based on GHA Guidelines for the RAP. The RMC will then be inaugurated. The committee will comprise of about ten (10) members to be selected from stakeholder group such as: one (1) each from GHA; EPA; District/Municipal Assemblies; an NGO nominated by the Ghana Association of Private Voluntary Organization in Development (GAPVOD); one from private sector; Ghana Union of Traders' Association (GUTA) or Association of Ghana Industries (AGI); ECG; GWCL; LVD.

In addition, about four (4) PAPs will be selected to serve on the committee. The details of the mandate of the RMC will be in the TOR. The RMC will be responsible for ensuring that the principles and specific requirements of the RAP are adhered to and implemented.

#### 7.4.2 Formation of Grievance Committee (GC)

One (1) Grievance Committee will be set up to minimize conflicts that would arise from the resettlement implementation activity. Membership of the GC will comprise nine (9) persons, one (1) from each of the stakeholder's group. In addition, four (4) PAPs will be selected. The GC Representatives will be vetted based on GHA Guidelines and then the Committee will be inaugurated. Letters will be sent out inviting officials.

**Grievance Procedures:** The objective of the Grievance Redress Procedure is to address and resolve grievances or complaints from affected persons promptly, fairly, and in a manner that is, to the extent possible, acceptable to all parties. The following principles will be applied: (i) provide straightforward and accessible ways for affected persons to voice complaints or resolve any disputes that might arise related to Project implementation; (ii) seek solutions to any tensions and conflicts early on, to avoid the use of a 'fire-fighting' approach; (iii) identify and implement appropriate and mutually acceptable actions to address complaints; (iv) respond in a timely manner, and with sensitivity to the needs of complainants; (v) ensure that claimants are satisfied with outcomes of the corrective actions, and maintain a dialogue with them to the extent possible; and (vi) avoid resorting to higher levels of adjudication, such as judicial proceedings, as much as possible.

All grievances will be received by the Grievance Committee (GC), through four (4) designated PAP representatives who are member of the Grievance Committee. The GC will have a membership of nine (9) drawn from GHA, the district/municipal assemblies within which the project falls. Four (4) PAPs will serve as the representatives for all the PAPs, one from each community and one representative from an NGO will be selected to complete the GC team. The GC will respond to any grievances that the PAPs may have during the RAP implementation. There will be two contact people from the GC to attend to all complaints as the first step by recording all complaints. The record will include name and contact of complainant, issue of complaint, date and time and receiving officer. The complainant will verify that the complaint lodged has been appropriately captured on the complaints sheet (to be designed and provided by GHA Environmental Unit). The grievances most likely to occur during the implementation of the RAP may include the following: (i) Disagreement over compensation amount assigned; (ii) Delays in receiving

compensation; (iii) Disagreement over asset ownership; (iv) Disagreement over proportionate sharing of assets with joint ownership.

The Committee will meet fortnightly to consider all lodged complaints and propose measures to address them. It is expected that all grievances would be addressed amicably at the committee level. However, in the unlikely case that an issue proves difficult to address, the complainant has the option of seeking redress at a higher level i.e. court of law and the committee members will co-operate. The details of each grievance will be recorded on a Grievance Form, along with name and address of the applicant, the application date, type of application, and the name of the officer receiving the grievance. A database of recording grievances along the lines described above will also be developed. In receiving the grievances of vulnerable affected persons (e.g., women, those above 65, the illiterate, or the physically challenged), the GHA Environmental Unit shall pay attention to any special needs, difficulties or concerns that they may have. The steps for grievance redress are as follows:

- Four (4) designated PAP representatives already serving on the GC shall receive grievances/complaints for the attention of the Grievance Committee. The GC shall inform the complainants on the status of their grievances within 7 days after the application.
- If the grievance can be resolved by the GC (i.e., if it necessitates no consultation with other organizations), possible corrective actions will be determined within 10 days. If resolution of grievance is seen to require commitment beyond the Grievance Committee, the members shall coordinate and consult with the relevant officials. In such cases, the time frame for the determination of possible corrective actions shall be 15 days.
- A complainant will be allowed to procure the services of an independent valuer at no cost to the PAPs, who would help that person determine an acceptable compensation. This can be presented to the committee as a grievance.
- The GC will investigate the complaints concerning processes.
- The Committee will communicate their proposed solution to the complainant and will also forward to GHA PCU the complaint, the outcome of investigations and their recommendations.
- After the case has been evaluated in detail and the possible corrective actions determined, the proposed solutions or corrective/preventive actions shall be discussed with the complainant.
- PAPs that are entitled to compensation or additional compensation will receive their entitlements from GHA Environmental Unit upon the recommendation of the Grievance Committee.
- Complaints that relate to the quantum of compensation will be referred to LVD to validate the compensation rates with the accompanying independent valuation report clearly stating the assumptions and rates used to arrive at compensation claims. The independent valuer of the Complainant would be invited to meet the LVD on an agreed date to resolve the issues involved in the disagreement. After negotiations, the LVD would communicate their recommendations to the GHA Environmental Unit which would in turn inform the claimant of the outcome.
- If the said GC recommends payment of the claim, then the GHA Environmental Unit would ensure that it is done before the structure under review is taken over. The acquiring entity, i.e. GHA is required to pay for the expenses involved in the PAP engaging his/her own valuer.
- Once an agreement has been reached between the applicant and the responsible party on the corrective actions, the applicant will be asked to sign off the grievance closeout form on his/her acceptance of the solution.
- If the applicant remains dissatisfied with the outcome, additional corrective action will be agreed on and carried out by the responsible party.

- Whenever a complainant's claim cannot be resolved satisfactorily, GHA Environmental Unit will procure the services of an arbitrator to mediate between the complainant and said committee. It is only after this mediation has failed that a claimant can then exercise the option of going to Court, as provided under Section 20 under Chapter 5 of the Constitution of the Republic of Ghana. The Committee has a maximum of 30 days following the application within which to resolve any grievances properly lodged with it. The applicant will then be informed by the designated officer and the corrective actions recorded in the Grievance Closeout Form.

#### 7.4.3 Formation of Committee to Oversee Payment

GHA will prepare a TOR for Committee to Oversee Payment of Resettlement Packages. Membership of the committee will be made up of nine (9) persons one (1) each from the stakeholder Group. In addition, four (4) PAPs will be selected. Letters will be sent out inviting officials.

#### 7.4.4 Community and Public Participation

Consultation with the project communities and stakeholders has been an important part of the RAP preparation for the Proposal. The key objectives of the consultation plan had been to inform the communities about the Proposal, with respect to resettlement /displacement issues, and to provide the local and broader community with opportunities to provide input into the RAP development document.

Moreover, all issues raised during the consultation process have been used in the refinement of old consultation plans and identification of new mitigation of resettlement/displacement strategies. By so doing, consensus building has been deepened.

#### 7.5 Grievance Redress Mechanism Levels

The GRM levels shall consist of a six-tier system:

- Local (sub-activity) level
- GHA internal dispute resolution system (GHAIIRS)
- Project-level grievance resolution
- Land Valuation Board referral
- National legal level
- Regional legal level

The general process is that a PAP should first raise a grievance at the local level. If it cannot be resolved at local level, it is referred to the GHAIIRS. If it is not resolved at this level, it is referred to the Grievance Redress Committee (GRC). If this proves unsuccessful in resolving the grievance, the complainant needs to procure the services of a valuer and submit a report to Land Valuation Division (LVD) of Lands Commission and ultimately the courts. The GRM described here is for PAP issues. Other interested parties have access to the project grievance redress mechanism system, described in the ESIA.

##### 7.5.1 Local Level Grievance Redress

The local-level grievance redress process will be supervised by the Resettlement Management Team (RMT) established in GHA but handled by the assigned officer in the GHA's project office on site. He/she

will receive, record name, contact, time of complaint. Simple enquiries will receive immediate response which shall be recorded, explained to PAP in a language they understand and signed (it could be thumb print for PAPs who cannot write) by both officer and PAP. This process will be facilitated by reference to the RAP database available to the officer-in-charge. In many cases, this will be possible because grievances will often relate to issues of inventories, requests for information, or to activities that have yet to take place, such as disbursement of compensation especially in relation to city ROWs.

When answers to enquiries cannot be provided immediately, the office will indicate this to the PAP and get information back to the PAP in three (3) days. Such information may be the response to the query or an invitation to meet the internal dispute resolution committee. It could also be because additional site visits and face-to-face discussions with the PAP are required to clarify the issues. If this visit results in a successful resolution, the PAP and the officer will sign the grievance redress form. In rural areas, PAPs who will need to travel long distances to make a complaint, will have Focal point persons appointed (a person who can read and write) to receive complaints and shall pass them on to the Project site office. Such focal point persons shall be resourced to commute or make phone calls. Where the grievance is lodged with a third party, they shall acknowledge receipt and be responsible for transferring the grievance form to the Project Office. Where these grievances are resolved directly, a simple grievance redress form will be signed by both the complainant and the recipient.

In urban operational areas complaints shall be received at the project site office or at GHA head office. In addition to project site office and GHA offices focal persons would be appointed in project areas. These persons like those in the rural areas will receive complaints that they shall forward to the Project Office. Such persons would be most useful to persons living with disability, old people with limited mobility and poor persons who cannot afford the expense of traveling long distances to lodge a complaint. Focal persons shall be resourced, so they commute to the project site offices when it is required and make phone calls. Where the grievance is lodged with a third party, they shall acknowledge receipt and be responsible for transferring the grievance form to the Project Office.

A simple grievance shall be resolved, and the grievance redress form shall be signed by both complainant and recipient. If the Officer at the site office cannot resolve the grievance, the complaint will be sent to GHA where all complaints shall be received by the Environmental and Social Performance Coordinator (ESPC). The ESPC will liaise with project directors and managers to resolve issues brought to her attention. When issues cannot be resolved at this level, then it shall be moved to the GHA Internal Dispute Resolution System (GHAIDRS) and the PAP shall be sent an invitation that details out, date, time, location and purpose of meeting.

#### 7.5.2 GHA Internal Dispute Resolution System (GHAIDRS)

The objective of this GHAIDRS is threefold:

- To reduce dependence on the LVD, whose statutory procedures for dealing with appeals again valuations can be time-consuming
- To provide a system for resolving issues related adequacy of compensation which is caused by not considering some components e.g. transport cost for chattels and hence
- To reduce the grievances related to valuations and compensation that must be considered by the Grievance Committee (GC).

The GHAIIRS panel will consist of the Environmental and social performance coordinator, (ESPC), Project director (PD) or his representative (specifically an engineer from the project development team), Social and Gender Assessment Officer (SGAO) and the Internal Audit officer. Compensation awarded by this panel shall be signed off by the Environmental and Social Performance Director (ESPD) and submitted for payment. An amount shall be fixed during the RAP preparation, beyond which approval will be sought from the Chief Operating Officer, the Internal Auditor and the Chief Financial Officer before it is paid. For larger claims, the panel will be chaired by a senior member of the GHA project management team (preferably the Chief Operating Officer). PAPs will have the right to be accompanied by their own valuer or representative who will be able to make representations on their behalf. The GHAIIRS will deal with the following types of grievances:

- Valuations of fixed assets, especially those that are of temporary construction. LVD will continue to deal with appeals related to valuations of land and permanent structures according to its statutory procedures.
- Compensation for improvements made by tenants that cannot be moved, such as windows, doors, internal modifications, utility connections.
- Compensation for loss of income.
- Disturbance/moving allowances.
- Landlord/tenant compensation related to advance payments and rent arrears.
- Supplemental assistance

### 7.5.3 Project Level Grievance Redress

The Grievance Committees (GC) will deal with all resettlement-related grievances that have not been resolved at either local level or through the GHAIIRS. The number of GCs to be established will depend on how scattered project activities are across space. PAPs must not incur high transport costs or walk long distances to get to a grievance committee meeting. At a minimum, there will be committees for project operating areas. The GCs will normally include a representative from each of:

- The local or municipal government representative
- A GHA ESP representative
- An NGO working in the area
- One PAP representing project sub-activity
- One PAP representing markets, if appropriate
- One PAP representing farmers, if appropriate
- One PAP representing economic enclave and any of these should be appointed by a trade association
- The LVD/Ministry of Agriculture (only in rural settings where farmland is affected)
- A valuer or a quantity surveyor shall sit on committee if the sub-activity is in an urban area

The GC should be chaired by the Environmental and Social Performance Coordinator (ESPC). Membership of the GC will be publicized throughout the sub-activity areas along with other details of the GRM. GCs will be mandated to provide a response within three (3) weeks of receiving formal notification of a grievance. In cases, where further site visits, investigations, or discussions with the PAP are deemed necessary in order to arrive at an amicable resolution, a date should be planned with the complainant for the follow-up visit which should fall within the mandated three (3) weeks. To facilitate the work of the Grievance Committee (GC), the RMT will supplement the grievance redress form with additional information relevant

to the complaint and include an assessment for consideration by the GRC of whether the grievance should be accepted, rejected, or partially accepted with or without additional enquiries/ investigations being undertaken. If the GC does not provide a satisfactory resolution for the PAP, he or she will be advised that they can seek redress through the legal system.

#### 7.5.4 Land Valuation Division

Should a PAP be dissatisfied with compensation offered for land or other assets on the land, the PAP shall be allowed to procure the services of a valuer who will assess his/her claim, prepare valuation report that shall be submitted to LVD. GHA shall also receive a copy of the report for record purposes. Land Valuation would set up a meeting with the Valuer of the PAP who will have to explain what the basis of his valuation is. After that if negotiations are required, LVD will negotiate and come to a value acceptable to both parties. Should the value be higher than what was agreed, the PAP will be paid the higher amount. The services of valuers procured by PAPs are paid for by the Agency that for whom the expropriation has been done. These negotiations usually take place after PAPs have received the rejected amount as part - payment so it has been impossible to retrieve monies paid when LVD negotiations have resulted in a lower compensation. Since the negotiations are done by valuers who represent both sides; the PAP, the Government and GHA would normally accept the value arrived at because it is accepted by both sides.

#### 7.5.5 National Legal Level

A PAP that is not satisfied after exhausting the project/LVD level grievance redress can under the laws of the Republic of Ghana seek redress from the court.

#### 7.5.6 Regional Legal Level

A PAP that is not satisfied after exhausting the project/LVD level grievance redress can under the laws of the Republic of Ghana seek redress from the court or proceed to the AfDB Independent Review Mechanism (IRM) for AfDB funded project (at the country office only).

#### 7.5.7 Contact Details of Persons Handling Grievance Redress

Currently, contact details of persons handling grievance redress between now and financial close (until the committees are formally put in place) are as follows:

- **Mr. Richard Kudjawu (0244659125) – GHA, N2 Project Coordinator**
- **Ms. Hilda Annan (0244991468) – GHA, N2 Project RAP Manager**
- **Mr. Kwaku Anim Boateng (0244657627) – KE&T, N2 Project RAP Consultant.**

### 8 Eligibility

Eligibility will be based on the category of losses to be suffered as a result of the roads project implementation process. The category of losses to be suffered will be derived from various interests and rights identified through common law and international conventions. Eligible affected persons will be presented in an Entitlement Matrix attached to this report as Table 59.0.



Moreover, eligibility will be based on the category of losses suffered as at the cut-off date was August 15<sup>th</sup>, 2015. Within the purview of the requirements of Ghanaian law and the Bank, these people are those who: (i) have legal rights to land, including customary and traditional rights (ii) do not have legal rights to land at the time of the census but have recognized rights of enjoyment or certain claims to the land or property; and (iii) occupy land although they have no legal right to it.

## 8.1 Proof of Eligibility

The GHA and LVD would consider various forms of evidence as proof of eligibility. The proof of eligibility will consider the following:

- Affected persons with formal legal rights documented in the form of land title registration certificates, leasehold indentures, tenancy agreements, rent receipts, building and planning permits, business operation licenses, utility bills among others.
- Unprocessed / unregistered formal legal documents will not bar eligibility.
- Affected persons will include: house owners; owners of residential/agricultural plots of land with no formal or recognized legal rights; business owners; tenants and land owners.

## 8.2 Entitlement Matrix

The entitlement matrix captures (Table 59) all affected parties, the characteristics of the impact, and the types of compensation/reinstatement due them. Categories in this table overlap because those being offered re-instatement fall into several groupings and the groupings tend to overlap. For instance, some of the owners of permanent and temporary structures own land as well. Again, some of the business operators also own property such as land or structures. The entitlement matrix identifies the following parameters:

### 8.2.1 Loss of Agricultural, Residential, Commercial or Institutional Land

For the legal title holders, customary usufruct rights holders', cash compensation of affected land will be paid since such land will be subjected to RoW acquisition and displacement. Moreover, there will be the need to identify all affected vulnerable encroachers or squatters for compensation for affected land. Also, all affected tenants will have to be compensated for unexpired duration of their lease.

### 8.2.2 Loss of Residential/Commercial (Business) or Institutional Structure

The owners of the affected structures with or without legal title, will be entitled to cash compensation at the replacement value (salvage value of the structures will not be deducted). In addition, an allowance will also be paid to the owners for the repairs of the remaining structures, if any.

### 8.2.3 Loss of Incomes

Business owners/operators, house owners/tenants, land lords / leaser who experience loss of incomes through loss of business income, loss of business rent, loss of residential rent and loss of advance rent for renters will be paid supplementary assistance. This assistance will be based on average net monthly profit, monthly rent passing, equivalent rent advance refund, monthly wages earned.

#### 8.2.4 Loss of Standing Crops

Where minor crop takings will evolve to affect standing crops, the affected cultivators will be entitled to cash compensation for the damaged crops. Compensation for the damaged crops will be calculated on current market prices.

#### 8.2.5 Loss of Trees

Vegetation clearing along RoW will result in the loss of trees both economic and non-economic. The owners of the affected trees, with or without land title, will be paid cash compensation, on current market value of the trees according to valuation outcome.

#### 8.2.6 Loss of Common Resources and Facilities

In case of the loss of any common resources or facilities, GHA will replace or restore the affected facility or resource in consultation with the affected community.

#### 8.2.7 Loss of/or Damage to Cultural Property

GHA will pay cash compensation for the replacement cost of cultural properties such as shrines, groves, cemetery, mausoleum, etc. Alternatively, GHA will construct the cultural property, in consultation with the affected community. GHA will also pay cash compensation for relocation of graves/shrines, mausoleums, etc.

#### 8.2.8 Loss of Public Infrastructure

GHA will pay cash compensation to the relevant agency based upon the loss or relocation cost of affected community infrastructure. Alternatively, the Contractor will replace or restore any damaged infrastructure to pre-project conditions or better, in consultation with the concerned agency.

**Table 59.0 Entitlement Matrix**

Type of Loss	Identification of Affected Assets	Entitlement	Details	Comments
Loss of Residential Structures/Accommodation/ Room	Owners of affected structures who are classified as eligible inhabitants	Compensation- capital market value of asset. Transportation rates for transfer of chattels or movable properties	Pay full cost for removal and fixing of movables, coverage of full cost for the transport expense for removal of belongings.	Compensation for demolished structures or civil improvement or compensation for relocation of structures.
Loss of Farm Buildings and other structures	Owners of affected structures	Compensation for the capital value of assets	Cash compensation provided for the value of assets.	-
Loss of Business (loss of business income; loss of business goodwill; loss of rented income; loss of wage income; loss of fees from trainees/apprentices)	Owners/operator (business owner/operator; landlord/leaser; business employees/attendants; trainer/person offering apprenticeship job training)	Supplementary Assistance based on (average net monthly profit; monthly rent passing; equivalent of rent advance to be refunded; monthly wages earned; training fees to be refunded) calculated for a specific period taking into consideration reinstatement period.	Cash compensation provided for the value of the materials	-
Loss of Residential Land	Various interest and rights -allodial title holder, freeholder, leaseholder, tenant licensee. Owner/Title Holder/Traditional Land Rights Holder of the affected plot	Compensation – capital market value of assets.	-	-
Loss of Agricultural Land	Owner/Title Holder/Traditional Land Rights Holder of the affected plot	Compensation – capital market value of assets	-	-
Loss of Food/Cash Crops and Loss of Economic Trees	Owners/Sharecroppers/Tenant Farmers/Licensees, Leasees	Open market value for assessed crops/economic trees	-	-
Loss of Cultural Sites, Graves, Shrines, Groves, etc.	Community at large	These sites remained in place	Cultural Sites, Graves and Shrines, etc., which have been identified remained in their locations as they are not located in any of the areas of development of roads infrastructure. If any relocation was required, Traditional Authorities will be involved in all decisions and will prescribe the appropriate methods and procedures to be followed.	No such sites have been identified within the project RoW alignment.

Assistance to Vulnerable Groups	Vulnerable Households	Additional Assistance as required to ensure no negative impacts.	GHA has in place a detailed program to identify and assist all vulnerable persons and households. Targeted support for livelihoods re-establishment or alternative livelihoods training. Interim payments if required during transition period and re-establishment of productive activities in resettlement community.	A committee will be formed to evaluate specific cases and designate those households eligible for Vulnerable Assistance.
Loss of Location for Temporary Structures – expense for moving structure	Owner of temporary structure, squatters	Supplementary assistance based on – transportation rates for the transfer of structure and disturbance allowance.	-	-

## 9 Valuation of and Compensation for Losses

Inspection and referencing of all bare land, buildings/structures and crops/farms falling within RoW was carried out initially on 22/11/2014. However, additional inspections and referencing of all affected assets within RoW due to new changes in alignments made by GHA were captured later. The purpose of the inspection and referencing of the affected properties was to collect the descriptive and other technical data on the affected properties. Both the internal and external areas were thoroughly inspected, except the areas which are not accessible. The existing buildings/structures were profiled and mapped out and measurements taken of all buildings and other structures. Additionally, profiles and mapping of bare lands and crops and farms including their measurements were undertaken.

### 9.1 Data Collection and Analysis

Information from the market were picked to assist in establishing appropriate unit rates for the assessment of the affected properties. Land sales in the immediate vicinity of the subject properties, and similar neighborhoods were also collected for analysis. Capital values of similar properties were also obtained for analysis to derive both unit rates for land and capital value of the properties. Construction Cost Data including prices of basic building materials and furnishes were compiled from the Open Market for analysis and compilation of unit rates for the assessment.

Basically, all the primary and secondary data collected were analyzed using computer applications such as Microsoft Excel Database and AutoCAD to establish a database for the valuation of the properties. Specific components to be completed include:

- The preparation of measured drawings of land and buildings to produce block plans;
- Preparation of schedule of accommodation and computation of floor areas; schedules shall be prepared to show the dimension, the gross internal and external areas of the accommodation;
- Lease documents, and available correspondence shall be analyzed to find out the unexpired terms since they have a bearing on the valuation.

### 9.2 Valuation Methodology

**Valuation for Residential/Commercial/Buildings/Structures:** The Full Replacement Cost Method is what is used in this FRAP for owners of permanent structures in line with Ghanaian and the AfDB requirements. The Capital Value of an existing development can be equated to the cost of reinstating the development on the same plot at current labor, material and other incidental costs; without any appropriate allowance for accrued depreciation with respect to physical functional and economic obsolescence. The rationale behind this trend is to afford affected owners the chance to replace their lost properties with new ones.

The first step shall be to establish the current Cost of Construction per square meter considering the architectural design, building heights, layout, floor spaces, plot size, fittings and fixtures and other physical attributes which have cost implications. The requisite information shall be obtained by analyzing the Construction Cost of Properties comparable to the subject Property. Necessary adjustments would be made to arrive at an appropriate rate for the subject Valuation. An amount of external work shall be added to the Cost of “brick and mortar” depending on existing development/improvements on the site.

**Assessing Replacement Cost of Civil Work Improvements:** In assessing the full replacement cost of civil works to be affected, the Replacement Cost Method was applied. This method involves the analysis of current construction cost of similar works. The Unit Construction Cost Rate adopted are as follows:

- **Mass Concrete Pavements** – GHc 13.00/m<sup>2</sup>
- **Pre-cast Concrete Paving Blocks** - GHc 50.00/m<sup>2</sup>
- **Short Sandcrete Walls** - GHc 20.00 to GHc 30.00/m<sup>2</sup>  
(Solid blocks or honey comb blocks)

**Unit Construction Rates Adopted:** To determine construction cost rates constructional details including stage of completion were noted and properties classified accordingly. The Replacement Cost Approach requires that the unit cost of construction for each category be determined and applied to the total external floor area of the building, length of affected fence walls and area of paved/landscaped. No account of depreciation is taken. See Tables 60.0, 61.0 and 62.0.

**Table 60.0 Construction Rates Adopted**

Serial number	Description	Minimum Unit Cost Rate per m (GHc)	Maximum Unit Cost Rate Per M (GHc)
1	Simple Sandcrete with Jalousies/Glass Louvres, Screed, Plywood, Aluminum/Asbestos Roof	500.00	750
2	Sandcrete with Sliding Glass, Ceramic/Porcelain, Timber/Plastic T&G, Tiles Roof/Long Span Roof Type	700	950
3	Combination of Wooden Frame & Sandcrete Block Walls, Shutters, Glass Louvres, Aluminum Roof, Doors	360	430
4	Typical Store Buildings – Sandcrete, No windows, Metal Gate	400	550

**Source: KE&T Data Records**

**Table 61.0 Categories and Construction Cost Rates for Fence Walls**

Serial Number	Description	Unit Cost Rate Per M (GHc)
1	Standard Sandcrete/Concrete Wall	100.00
2	Standard Sandcrete/Concrete Wall with Metal	90.00

3	Barded Mesh/Chain Link Fencing	50.00
4	Combined – Sandcrete Wall & Chain Link	70.00
5	Timber Fence Wall	70.00

Source: KE&T Data Records

**Table 62.0 Categories and Construction Cost Rates for Pavements**

Serial Number	Description	Unit Cost Rate Per M (GHc)
1	Mass Concrete Pavement	60.00
2	Bitumin/Surface dressed Pavement	90.00
3	Pre-Cast Concrete Paving Blocks/Slabs	80.00
4	Loose Gravel/Chippings	20.00

Source: KE&T Data Records

**Valuation for Agricultural/Residential/Commercial/Institutional/Land:** The Value of Land shall be determined by comparing market evidence on recent sale of plots in the vicinity or similar neighborhoods. The rate per acre/hectare shall be established as a basis for assessing the Land Value for the subject property taking into consideration the differences in attributes between the subject property and the comparable with regards to the specific advantages of the location.

**Valuation for Affected Land Areas:** For the purpose of this study, land areas affected are the measured land area occupied by the affected building, civil works, farms, business and institution. This study is limited to these affected areas and covers the boundaries of the land as shown by reputed owners.

The study did not assess the leaseholder's term subsisting in their affected land. The study assumed that these are leases held on long term leases and accordingly a lease hold period of 50 years was adopted. For this reason, the assessed land values are presented separately from the building values.

The reversionary interest in these affected lands were not assessed and thus provision for compensation as estimated under the study covers leaseholders and does not include compensation to the freeholder or allodial interest holder namely the chiefs and their respective families. Land compensation at replacement values, however, will be provided to all persons with a legitimate claim. Government of Ghana procedures will be followed and funds equivalent to the estimated value of lands will be held in special account as agreed with GHA for disbursement to landowners once LVB has confirmed the land claim.

**Valuation for Crops/Farms:** The value of crops/farms shall be determined using the official district/municipal LVD approved compensation rates, plus disturbance allowance. Annual crops and seasonal crops will not be compensated as PAPs will be allowed to harvest before the project implementation. However, in order to prevent financial impact or loss of harvest that mainly includes food crops, notice will be provided to all PAPs with seasonal crops to alert them when to stop cultivating once they have been compensated for loss of land. Seasonal crops that take less than six (6) months to reach maturity that allows them to be fully harvested and the land cleared will include maize, vegetables, okra, beans.

**Crops Compensation Rates:** GHA adopted the Crop Rate Negotiation Committee on the crops compensation rates for the different categories of crops as outlined in Tables 63.0, 64.0, 65.0, 66.0

**Table 63.0 Cash Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Cocoa	18.00	14.20	10.00	6.80	8,100.00	6,390.00	4,500.00	3,060.00
Oil Palm	73.50	45.50	25.50	9.25	4,410.00	2,730.00	1,530.00	555.00
Citrus	38.60	15.30	10.10	6.00	4,246.00	1,683.00	1,111.00	660.00
Coffee	3.90	2.85	1.65	1.20	1,719.00	1,256.85	727.65	529.20
Kola	30.00	15.00	11.00	6.75	1,800.00	900.00	660.00	405.00

Source: GHA Data Records

**Table 64.0 Tree Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Teak	32.20	24.00	10.40	6.40	14,200.00	10,584.00	4,586.40	2,822.40
Moringa	9.00	7.60	2.20	1.50	4,050.00	3,420.00	990.00	675.00
Cinnamon	6.50	4.90	1.70	0.85	2,925.00	2,205.00	765.00	385.50
Acacia	25.80	4.00	1.50	1.00	1,806.00	280.00	105.00	7.00
Cedrella	28.20	24.00	9.90	6.20	12,436.20	10,584.00	4,365.90	2,734.20

Source: GHA Data Records

**Table 65.0 Fruit Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Pawpaw	3.50	2.00	1.20	0.75	1,575.00	900.00	540.00	337.50
Cashew	38.00	27.00	18.00	12.00	2,660.00	1,890.00	1,260.00	840.00
Avocado	38.00	27.00	18.00	12.00	2,660.00	1,890.00	1,260.00	840.00
Mango	45.00	29.00	17.00	9.00	2,835.00	1,827.00	1,071.00	567.00
Guava	36.00	27.00	17.20	11.50	2,520.00	1,890.00	1,071.00	805.00
Dawadawa	13.00	8.00	3.70	1.50	910.00	560.00	259.00	105.00
Pineapple	3.58	3.00	1.00	0.50	4,654.00	3,900.00	1,300.00	650.00
Sugarcane	4.30	2.20	1.20	1.00	5,160.00	2,640.00	1,440.00	1,200.00
Apple	36.00	27.00	17.20	11.50	2,520.00	1,890.00	1,204.00	805.00
Watermelon	13.00	7.00	3.85	2.60	3,900.00	2,100.00	1,155.00	780.00
Coconut	38.00	26.50	17.70	11.90	2,280.00	1,590.00	1,062.00	714.00
Banana	10.10	4.00	2.20	1.00	4,545.00	1,800.00	990.00	450.00

Source: GHA Data Records

**Table 66.0 Vegetable Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Tomatoes	1.41	0.67	0.51	0.35	1,410.00	670.00	510.00	350.00
Pepper	2.35	1.20	0.80	0.65	1,880.00	960.00	640.00	520.00
Carrot	1.40	0.70	0.50	0.38	4,200.00	2,100.00	1,500.00	1,140.00
Okro	1.50	0.60	0.50	0.30	1,500.00	600.00	500.00	300.00
Garden Eggs	1.57	0.65	0.45	0.30	1,256.00	650.00	450.00	300.00
Onions	1.25	0.63	0.43	0.32	1,375.00	756.00	516.00	384.00
Ginger	2.50	1.35	0.85	0.65	1,750.00	1,080.00	680.00	520.00

Source: GHA Data Records



**Table 67.0 Food Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Cassava	3.20	1.50	1.00	0.70	1,440.00	900.00	600.00	420.00
Yam	4.90	4.00	1.90	1.60	1,470.00	1,200.00	570.00	480.00
Plantain	12.00	8.10	3.10	1.25	5,400.00	3,645.00	1,395.00	562.00
Cocoyam	2.40	1.75	1.00	0.80	1,200.00	875.00	500.00	400.00

Source: GHA Data Records

**Table 68.0 Cereal Crops Compensation Rates**

Crops	Mature (per unit rate GHc)	Medium (per unit rate GHc)	Small (per unit rate GHc)	Seedling (per unit rate GHc)	Mature (per acre rate GHc)	Medium (per acre rate GHc)	Small (per acre rate GHc)	Seedling (per acre rate GHc)
Maize	0.51	0.43	0.20	0.15	1,031.73	869.89	404.60	303.45
Cowpea (Beans)	1.00	0.70	0.35	0.30	2,023.00	1,416.10	708.05	606.90
Groundnut	0.80	0.50	0.20	0.07	2,400.00	1,250.00	800.00	350.00
Millet/Sorghum	0.50	0.32	0.21	0.15	1,100.00	704.00	462.00	330.00
Rice (Rain-fed)	-	-	-	-	1,400.00	520.00	315.00	200.00

Source: GHA Data Records

**Valuation for Loss of Business/Income:** Loss of profit allowance shall be assessed by establishing net profit per month multiplied by the duration of construction. Loss of profit will be net profit per month multiplied by the estimated construction period. The possible loss of the business' Net Profit during the relocation of structures and commencement of business at new sites were also assessed and applied. Key factors considered were the nature of business, location attributes and the commercial characteristics of the project roads corridor. Net income was assessed based on a reinstatement period of six (6) months.

Based on field income data obtained from the survey and based on the professional opinion of the Valuer, a representative amount was adopted for the various categories of affected businesses. Provision was made for structure owners who reside in their structures for any incidental cost likely to arise from the impact (permanent residential accommodation). See Table 69.0.

**Table 69.0 Business Category and Recommended Net Income**

Serial Number	Category	Net Profit (GHc)
1	Petty Trade (Groceries/Hair/Cosmetics/Sewing/Book)	160.00
2	Communication Center/Internet Cafe	160.00
3	Hairdressers, seamstress/Tailor, Barber, etc.	200.00
4	General Retail Household Items – Mattresses, Plastics, Second Hand Household Goods	240.00
5	Hardware – (Cement, Electrical, Plumbing, Building, Timber)	240.00
6	Key Cutters, Shoe makers Shop	160.00
7	Hospitality Facilities (Chop bars, Fast Food, Restaurant, Drinking Café)	200.00
8	General Repairs – TVs, Fridges, etc.	200.00

9	Carpentry, Aluminum, Framed Products, Welding, Mechanics Workshops	240.00
10	Vehicle Sales, Accessories, Garages/Auto	200.00
11	Residence	60.00 per month (rent for single room)
12	Sale of Lotto Tickets, sale of Cell Phone Units	80.00

**Source: KE&T Data Records**

**Assessment of Relocation Cost (Transportation Cost):** The size and type of structure were the key factors influencing the cost of relocating. Data obtained show that cost of transporting within the project roads corridor is standard and is charged by type/size of the load transported rather than by the distance.

Analyzed transport Cost Data adopted ranged from GHc200.00 to GHc 300.00 depending on the size and type of structure. For the purpose of standardization, sizes of the structure were categorized using the Shipping Container measurements. Using the length of structure, size categorization adopted is:

- Small, less than or equal to 20-footer (20 feet) container.
- Medium, more than 20 feet but less than 40 feet.
- Large, more than 40 feet.

The following were adopted as Cost for fork lifting and transporting structures. See Table 70.0.

**Table 70.0 Cost of Forklifting and Transporting Structures**

Small Size	Medium Size	Large Size	Others (Canopies, Dismantled Sheds, Extra Small Structures)
GHc100.00	GHc 125.00	GHc 150.00	GHc 50.00

**Source: KE&T Data Records**

**Valuation for Common Resources and Facilities/Cultural Properties/Public Infrastructure:** Replacement values shall be sought and general estimates from the construction industry will be used to calculate the expected values.

**Supplemental Assistance to Vulnerable People and Owners of Temporary Structures:** Each person falling into this category as defined above will receive an additional amount. This additional amount will be equivalent to 15% and 20% of the value of the compensation to be provided, respectively for vulnerable and very vulnerable persons. Moreover, supplemental assistance in the form of cash is provided to Owners of temporary structures. It covers the following:

- Cost of removal and transporting structures (Relocation Cost).
- Loss of Business during the transition period of relocating.
- Loss of Business Goodwill.
- Loss of connection to Utilities (electricity).
- Loss of mass concrete floors, paved frontage and other civil work improvements.

**Disturbance:** In addition to the assessment of Land and Buildings, Disturbance will also be assessed. Disturbance as interpreted under section 7 of the State Lands Act is the “reasonable expenses incidental to any necessary change of residence or place of business”. By implication, this represents the expense and other losses above the Value of Land and covers the expenses of moving to alternative premises, cost incidental to removal such as loss of fixtures. In the case of owner-occupiers of business, disturbance will in addition, cover the injury to the goodwill suffered by the removal to alternative premises, the Loss in Value, caused by the forced sale of stock, and the Loss of Profits. Another form of disturbance that will be added to the value of the properties is professional fees for Valuers and Lawyers who may be engaged by the property affected persons to advise them on their claims for compensation.

Each PAP will be paid an additional sum of money for the loss of business goodwill, reconnection to electricity, moving of stock/goods and other intangible losses arising out of the acquisition and for disturbance caused to the user in his/her use and enjoyment of the property. Disturbance was assessed as a percentage of cost and it ranged from **2% to 10%** of the Total Supplementary Assistance. Disturbance margin adopted reflect the category of utility services available, business, size and general business environment as observed at the business premises.

Typical incidental expenses associated with the loss of building identified included:

- Cost of alternative accommodation for owner-occupiers and tenants during the reinstatement period.
- Possible loss of tenants by landlords i.e. landlords’ disturbance arising from refunding rents collected/providing tenants with alternative accommodation, loss of tenant rent during reinstatement period.
- Relocation expenses for landlord and tenants occupying affected property.

Based on the Valuer’s experience and professional opinion, the following was adopted:

- 10% of the Full Replacement Cost for partly affected properties.
- 15% of the Full Replacement Cost for fully affected Residential properties.
- 20% of Full Replacement Cost for fully affected commercial and mixed-use properties.

**Containers/Kiosks:** Payment of supplemental assistance to owners of temporary structures will also be made. The assessment of Supplemental Assistance is based on the premise that Temporary Structures could be moved and relocated. Anticipated cost of transporting/moving the structures to the desired destination would be related to the size of the structure and assumed reasonable location. In addition to the cost of moving the structure, all temporary structures which have concrete/sandcrete bases will be assessed to include the concrete/sandcrete works.

## 10 Identification and Selection of Resettlement Site, Site Preparation and Relocation

All PAPs will not have to be relocated on a specific relocation site. Therefore, there are no measures to be taken to address this concern. Invariably, the issue with identification and selection of resettlement site, including site preparation and relocation do not apply to this Proposal. Data from the socio-economic survey indicated that majority of the PAPs preferred mode of compensation was cash. See Tables 71.0, 72.0 and 73.0.

**Table 71.0 Preferred Mode of Compensation on Section 1 Project Road**

<b>Mode of Compensation</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>	<b>Total (Frequency)</b>	<b>Total (%)</b>
Cash Compensation	341	78.0	184	76.0	536	79.0
Lana-for-Land	22	5.0	5	2.0	27	4.0
Resettlement at New Location	4	1.0	2	1.0	14	2.0
Replace Damaged Structures Sections	4	1.0	5	2.0	7	1.0
Resettlement and Cash Compensation	13	3.0	15	6.0	20	3.0
Govt Should Decide	44	10.0	24	10.0	61	9.0
Undecided	9	2.0	7	3.0	14	2.0
<b>Total</b>	<b>437</b>	<b>100.0</b>	<b>242</b>	<b>100.0</b>	<b>679</b>	<b>100.0</b>

**Source: KE&T Data Records**

**Table 72.0 Preferred Mode of Compensation on Section 3 Project Road**

<b>Mode of Compensation</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>	<b>Total (Frequency)</b>	<b>Total (%)</b>
Cash Compensation	158	77.0	74	76.2887	241	80.0664
Lana-for-Land	8	4.0	5	5.1546	6	1.9934
Resettlement at New Location	4	2.0	3	3.0928	4	1.3289
Replace Damaged Structures Sections	4	2.0	2	2.0619	2	0.6645
Resettlement and Cash Compensation	8	4.0	4	4.1237	3	0.9967

Govt Should Decide	20	10.0	3	3.0928	40	13.2890
Undecided	2	1.0	6	6.1856	5	1.6611
<b>Total</b>	<b>204</b>	<b>100.0</b>	<b>97</b>	<b>100.0</b>	<b>301</b>	<b>100.0</b>

Source: KE&T Data Records

**Table 73.0 Preferred Mode of Compensation on Section 2 Project Road**

<b>Mode of Compensation</b>	<b>Male (Frequency)</b>	<b>Male (%)</b>	<b>Female (Frequency)</b>	<b>Female (%)</b>	<b>Total (Frequency)</b>	<b>Total (%)</b>
Cash Compensation	212	79.1045	122	76.2500	342	79.9065
Lana-for-Land	6	2.2388	2	1.2500	6	1.4019
Resettlement at New Location	2	0.7463	3	1.8750	3	0.7009
Replace Damaged Structures Sections	3	1.1194	7	4.3750	2	0.4673
Resettlement and Cash Compensation	5	1.8657	6	3.7500	20	4.6729
Govt Should Decide	30	11.1940	5	3.1250	40	9.3458
Undecided	10	3.7313	15	9.3750	15	3.5047
<b>Total</b>	<b>268</b>	<b>100.0</b>	<b>160</b>	<b>100.0</b>	<b>428</b>	<b>100.0</b>

Source: KE&T Data Records

## 11 Environmental Protection

Environmental protection will be ensured in accordance with Environmental and Social Management Plan (ESMP) prepared as a stand-alone document. Moreover, environmental protection measures will be implemented as contained in the Environmental and Social Impact Assessment (ESIA) report.

## 12 Implementation Schedule

All assets acquisition for the roads project will be completed before the start of construction work at sections where resettlement issues are envisaged. And all such land sections will be provided free of encumbrances. All affected assets owners will be given adequate notice to vacate those drainage sections with affected assets. The implementation schedule for the roads project is given in a matrix format in **Table 74.0**. It should be noted that implementation periods shall change depending on when the financial resources shall be finalized. However, the change in dates shall not affect the actual periods. Each step shall not necessarily change. It is important that all RAP activities including payments for compensation are completed before roads project construction begins. This is to ensure that all possible hindrances to the project implementation will have been dealt with. However, some grievances may arise during the project implementation.

**Table 74.0 RAP Implementation Schedule**

Task Number	Task/Activity	Year 1	Year 2	Responsible Agency
1	Completion of Draft RAP	Month 1	-	GHA/AfDB Personnel
2	Approval of Draft RAP	Month 4	-	GHA/AfDB
3	Training of GHA Staff in RAP Implementation	Month1 through Month 4	-	AfDB Resettlement Specialist
4	PAP Community Consultation	Month1 through Month 4	Month 1 through Month 6	Community Liaison Team/RAP Coordinating Unit
5	Notification of Entitlements/Negotiation Process	Month 4	-	Community Liaison Team/RAP Coordinating Team
6	Payment of Compensation	Month 4	Month 1 through Month 5	Compensation and Payment Team
7	Notification to PAP/Community of Demolition of Structures	Month 3 and Month 4	Month 1 through Month 4	Community Liaison Team/RAP Coordinating Team
8	Demolition of Structures	Month 4	Month 1 through Month 5	GHA/Contractors
9	Replace any Community Facility (water stand pipe, borehole, well, etc.)	-	Month 1 through Month 5	GHA/Contractors
10	Setup and Implement Community Schemes	Month 2 through Month 4	Month 1 through Month 8	Community Liaison Team/Project Coordinating Team
11	Grievance Mechanism and Procedures	Month 2 through Month 4	Month 1 through Month 8	Community Liaison Team
12	Performance Monitoring	Month 2 through Month 4	Month 1 through Month 8	Resettlement Committee Unit
13	External Evaluation (bi-annual for 3 years)	-	Month 8	Independent Evaluator
14	RAP Audit and Closure	-	Month 8	Resettlement Committee Unit

Source: KE&T Data Records

### 13 Costs and Budget

Buildings and structures, crops and farms, bare farmlands are the assets which will be acquired through involuntary resettlement and displacement by the project roads implementation program. Summary of interim values for compensation payment are indicated in Tables 75.0, 76.0, 77.0 and 78.0. (See ESIA Report page 179).

**Table 75.0 Summary of Interim Values for Compensation for Section 1: Asutuare Jn-Volivo Road (28.7km)**

Serial Number	Description	Number of PAPs	Land Value (GHc)	Building, Structures Value (GHc)	Crops, Farms Value (GHc)	Compensation Sum (GHc)
1	Buildings, Structures	134	1,498,471.88	6,034,690.32	-	7,533,162.19

2	Crops, Farms	278	-	-	718,539.98	<b>718,539.98</b>
3	Bare Land (427.67 acres)	295	1,451,389.00	-	-	<b>1,451,389.00</b>
4	<b>Sub-Total</b>	<b>707</b>	<b>2,949,860.88</b>	<b>6,034,690.32</b>	<b>718,539.98</b>	<b>9,703,091.17</b>

Source: GHA Data Records

**Table 76.0 Summary of Interim Values for Compensation for Section 3: Dufor Adidome-Asikuma Jn Road (38.4km)**

Serial Number	Description	Number of PAPs	Land Value (GHc)	Building, Structures Value (GHc)	Crops, Farms Value (GHc)	Compensation Sum (GHc)
1	Buildings, Structures	60	320,553.78	3,339,160.26	-	<b>3,659,714.03</b>
2	Crops, Farms	120	-	-	547,318.58	<b>547,318.58</b>
3	Bare Land (427.67 acres)	134	1,596,390.00	-	-	<b>1,596,390.00</b>
4	<b>Sub-Total</b>	<b>314</b>	<b>1,916,943.78</b>	<b>3,339,160.26</b>	<b>547,318.58</b>	<b>5,803,422.61</b>

Source: GHA Data Records

**Table 77.0 Summary of Interim Values for Compensation for Section 2: Asutuare-Aveyime Road (24.0km)**

Serial Number	Description	Number of PAPs	Land Value (GHc)	Building, Structures Value (GHc)	Crops, Farms Value (GHc)	Compensation Sum (GHc)
1	Buildings, Structures	204	399,835.82	5,222,336.46	-	<b>5,622,172.28</b>
2	Crops, Farms	215	-	-	105,573.63	<b>105,573.63</b>
3	Bare Land (427.67 acres)	35	92,150.00	-	-	<b>92,150.00</b>
4	<b>Sub-Total</b>	<b>454</b>	<b>491,985.82</b>	<b>5,222,336.46</b>	<b>105,573.63</b>	<b>5,819,895.91</b>

Source: GHA Data Records

**Table 78.0 Summary of Total Interim Values for Compensation for Sections 1,3 and 2: ( Asutuare Jn-Volivo Road (28.7km); Dufor Adidome-Asikuma Jn Road (38.4 km); Asutuare-Aveyime Road (24.0 km)**

Serial Number	Description	Number of PAPs	Land Value (GHc)	Building, Structures Value (GHc)	Crops, Farms Value (GHc)	Compensation Sum (GHc)
1	Buildings, Structures; Crops, Farms, Bare Land <b>(Total</b>	1,475	5,358,790.47	14,596,187.04	1,371,432.19	<b>21,326,409.69</b>

	<b>Acreage – 1,045.39)</b>					
2	GHA Operation Expenses	-	-	-	-	<b>306,000.00</b>
3	<b>Grand Total</b>	<b>1,475</b>	<b>5,358,790.47</b>	<b>14,596,187.04</b>	<b>1,371,432.19</b>	<b>21,632,409.69</b>

Source: GHA Data Records

The overall cost for implementing the RAP is **GHc 26,292,463.89** of which **GHc 4,563,154.89** is earmarked for RAP implementation and monitoring. The bulk of this cost is the full replacement costs of immovable permanent structures within the road reservation for which some demolition is triggered. The RAP costs are expected to be met by Ghana Government through Ministry of Finance and hence be budgeted for accordingly. See Tables 79.0 and 80.0.

**Table 79.0 RAP Asset Entitlement Budget**

Serial Number	Category Description	Compensation Item	Entitlement (GHc)	Comments
1	Assets Entitlement	Buildings/Structures/ Crops/Farms/Bare Land	21,326,409.69	Valuation Data
2	Assets Entitlement	GHA Operation Expenses	306,000.00	Valuation Data
3	Assets Entitlement	Provision for Supplemental Assistance to Vulnerable PAP	11,700.00	Valuation Data (10% affected assets value for 39 PAPs)
4	Assets Entitlement	Provision for Supplemental Assistance to Very Vulnerable PAPs	85,200.00	Valuation Data (15% affected assets value for 213 PAPs)
5	<b>Assets Entitlement</b>	<b>Sub-Total</b>	<b>21,729,309.00</b>	-

Source: GHA Data Records

**Table 80.0 RAP Implementation Budget**

Serial Number	Category Description	Compensation Item	Entitlement (GHc)	Comments
1	Implementation Budget	Total Cost of Compensation/ Entitlement	21,729,309.00	Total Assets Entitlement Cost
2	Implementation Budget	RAP Administration Cost	1,303,758.54	6% of Entitlement Cost
3	Implementation Budget	RAP Monitoring Cost	2,172,930.90	10% of Entitlement Cost
4	Implementation Budget	RAP Audit and Closure	434,586.18	2% of Entitlement Cost
5	Implementation Budget	Allowance for Inflation and Contingencies	651,879.27	3% of Entitlement Cost
6	Implementation Budget	Sub-Total	4,563,154.89	Addition of Serial Number Line Items (2+3+4+5)



7	Implementation Budget	Total	26,293,463.89	Addition of Serial Number Line Items (1+6)
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Source: GHA Data Records

## 14 Monitoring and Evaluation

**Introduction:** Project monitoring and evaluation will include internal and external monitoring, AfDB supervision missions, a mid-term review and a final evaluation incorporating the completion report of the implementing agencies. GHA will supervise the Implementation Program and ensure the timely execution of roads project activities. To enable GHA effectively undertake the monitoring, a Resettlement Monitoring Committee will be set up at the beginning of the implementation period to coordinate the various aspects of the resettlement / displacement process. RMC meetings will be held every month for the initial resettlement period of say eight (8) months, thereafter quarterly for the rest of the project implementation period. GHA will co-chair the committee with the Civil Society Representative.

This project is adopting a participatory monitoring and evaluation strategy where all stakeholders will be involved to monitor the various stages of resettlement and jointly evaluate the FRAP after all the processes have been completed. Specific indicators shall be monitored to ensure that notices are served in a timely manner to allow property owners to vacate the RoW immediately after community sensitization and public awareness. Monitoring plans will be developed to cater for all three (3) phases of the project implementation program: (i) monitoring during pre-construction (ii) monitoring during construction (iii) monitoring during operation.

### 14.1 Monitoring Objectives

Objectives of the Monitoring Program are:

- To ascertain that the principles and the specific requirements of the RAP are fully implemented.
- To ensure that PAPs are successfully relocated to avoid and reduce impoverishment.
- To ensure that difficulties facing PAPs are identified and addressed.
- To ensure that records of experiences are kept for future references.

### 14.2 Internal Monitoring and Supervision

Internal Monitoring of the resettlement operations will be undertaken by GHA and LVD following the schedules in the RAP. The day-to-day field supervision will be the responsibility of the GHA Resettlement Office. A record of activities will be captured in the Monthly and Quarterly Progress Reports (QPR) which are subject to review by RMC. The RMC will continuously take stock, discuss reports received and assess solutions proposed. Their activities will ensure that the resettlement is successfully implemented. Regular Quarterly Reports will be produced and submitted to GHA. GHA will bear the cost of monitoring.

### 14.3 External Monitoring and Supervision

The presence of representatives from civil society on NGO's and the private sector on the RMC will ensure on-going external monitoring and supervision. In this way, the resettlement of affected persons will be facilitated and GHA could measure the success or otherwise of the RAP.

## 14.4 Monitoring Indicators

As a means of effectively reporting on the RAP implementation, the following indicators will be watched during project implementation as presented in a matrix format in **Table 81.0**

**Table 81.0 Monitoring Indicators**

Date	Activity	Monitoring Indicator	Means of Verification
First to second months into the project implementation.	Ascertain arrangement for compensating PAPs.	Demolition/Removal of structures; disconnection of utility services; closure of businesses.	Monitoring completed; report submitted.
Second to third month into the project implementation.	Supervise compensation payment of PAPs.	PAPs compensated or being assisted; assistance offered.	PAPs traced to new locations; report submitted.
Third to Fourth month into the project implementation.	Meeting with PAPs to find out any challenges.	PAPs compensated; businesses restarted (where necessary).	Interaction completed; problems identified; solutions suggested; report submitted.
Fourth to Fifth month into the project implementation.	Follow up meeting to see how challenges are resolved.	PAPs compensated; businesses restarted (where necessary).	Interaction completed; problems identified; solutions suggested; report submitted.
Fifth month to the end of the project (monthly).	Determine how far the livelihood of PAPs has been restored.	Business on-going; restored or better.	Turnover/sales profit figures submitted as proof; report submitted.

**Source: K E & T Data Records**

## 14.5 Post-Project Evaluation

In addition to the periodic evaluation that will be carried out during the planned project period, an audit will be done upon project completion. The audit will undertake the following:

- Appraise the extent of the achievement of the resettlement activities.
- Gauge the satisfaction level of PAPs.
- Measure the progress of persons who have been compensated.
- Access the sufficiency of the planned actions carried out.

The following method will be adopted to achieve the audit objectives:

- Public Fora
- Completion audit of available PAPs.

➤ **Public Fora**

Quarterly meetings will be held with all PAPs. Extensive use of the print and electronic media will ensure that as many PAPs as possible will attend. The meetings are expected to mobilize as many PAPs as possible and give a common voice to affected persons.

➤ **Completion Audit**

An audit will be held to determine whether the efforts to restore the living standards of the affected population have been properly designed and executed. This completion audit will verify that all physical inputs earmarked in the RAP have been designed and all services provided. The audit will also evaluate if the mitigation actions prescribed in the RAP have been effective. The baseline conditions of the affected parties before compensation payment will be used as a measure against their socio-economic status after the resettlement/displacement scheme.

Questionnaires will be administered to as many PAPs as possible. In addition to the indicators used in the baseline survey, the questionnaires will also identify issues such as:

- Extent of recovery of business.
- Structures being used for business.
- Any expansion or shrinkage.
- Problems encountered.
- General perception of compensation payment.

To be effective, the completion audit will take place after all RAP activities have been completed including development initiatives, but before the financial commitments to the program are finished. This will allow the flexibility to undertake any corrective action that the auditors will recommend before the project is completed.

## 15 Funding and Disclosure

### 15.1 Funding

Fund of resettlement activities will be provided by the Ghana Government, through the Ministry of Finance and Economic Planning, the Ministry of Transport and then through GHA. Ghana Government will be supported by JICA and AfDB.

### 15.2 Disclosure

GHA will disclose the RAP to all stakeholders at the commencement of the proposed roads project. A summary of RAP will be prepared specifically for this purpose and translated into local languages and provided to the affected communities (and kept at the roads project sites). This will ensure that the local communities are aware of the roads project benefits and provisions available for various types of PAPs, as given in the entitlement framework. In addition, the list of all affected persons and affected assets/properties will be published in the major newspapers and official websites of GHA, EPA, MoT and LVD.

## References

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2. AfDB Environmental Policy (2004)
3. AfDB Gender Policy (2001)
4. AfDB Bank Group on Poverty Reduction (2004)
5. AfDB Integrated Safeguard on Involuntary resettlement (2013)
6. AfDB Checklist for Gender Mainstreaming in the Infrastructure Sector (2009)
7. AfDB Handbook on Stakeholder Consultations and Participation in ADB Operations (2001)
8. AfDB Integration Safeguard System Working Progress (2012)
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10. AfDB, Manzini to Mbadlane (MR3) Highway Project, Abbreviated Resettlement Action Plan Summary (2014). The kingdom of Swaziland.
11. AfDB, Isebania-Kish-Ahero Road Rehabilitation Project, Resettlement Action Plan Summary (2015). Republic of Kenya.
12. AfDB, Nairobi Outer-Ring Road Improvement Project, Resettlement Action Plan Summary (2013), Republic of Kenya.
13. AfDB, Accra Urban Transport Project Phase 1, Full Resettlement Action plan (2015). Republic of Ghana.

APPENDIX A PHOTOS OF ESIA/RAP REPORTS STAKEHOLDER ENGAGEMENT EDUCATION



ESIA/RAP Reports Stakeholders Engagement Education at Asutuare Jn (Section 1 Project Road)



ESIA/RAP Reports Stakeholders Engagement Education at Asutuare Jn (Section 1 Project Road)





**ESIA/RAP Reports Stakeholders Engagement Education at Asutuare Jn (Section 1 Project Road)**



**ESIA/RAP Reports Stakeholders Engagement Education at Asutuare Jn (Section 1 Project Road)**





ESIA/RAP Reports Stakeholders Engagement Education at Asutware Jn (Section 1 Project Road)



ESIA/RAP Reports Stakeholders Engagement Education at Asutware Jn (Section 1 Project Road)





**ESIA/RAP Reports Stakeholders Engagement Education at Asutware Jn (Section 1 Project Road)**



**ESIA/RAP Reports Stakeholders Engagement Education at Asutware Jn (Section 1 Project Road)**





**ESIA/RAP Reports Stakeholders Engagement Education at Asutwae Town (Section 2 Project Road)**



**ESIA/RAP Reports Stakeholders Engagement Education at Asutwae Town (Section 2 Project Road)**





**ESIA/RAP Reports Stakeholders Engagement Education at Volivo (Section 2 Project Road)**



**ESIA/RAP Reports Stakeholders Engagement Education at Volivo (Section 2 Project Road)**



**ESIA/RAP Reports Stakeholders Engagement Education at Aveyime (Section 2 Project Road)**





**ESIA/RAP Reports Stakeholders Engagement Education at Dufor Adidome (Section 3 Project Road)**





**ESIA/RAP Reports Stakeholders Engagement Education at Dufor Adidome (Section 3 Project Road)**

## APPENDIX B

## INTERIM BUDGET ON COMPENSATION COSTS FOR PAPs ASSETS

<b>CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)</b>				
<b>INTERIM COMPENSATION ON AFFECTED BARE LANDS</b>				
<b>NO</b>	<b>TOWN</b>	<b>NAME</b>	<b>FARM/LAND SIZE (ACREAGE)</b>	<b>LAND VALUE (GH¢)</b>
1	<b>ADIDOME</b>	AGNES BUSSI	0.28	840.00
2		ZIGI KWAME PETER	0.35	1,050.00
3		AGNES AKUA NUKPEDU	0.42	1,260.00
4		NUKPEDU CHARLES	0.27	810.00
5		UNKNOWN	0.17	510.00
6		MARY KWAO	0.40	1,200.00
7		AGIETOR WISDOM	1.09	3,270.00
8		SENYO MENSAH	0.40	1,200.00
9		KWASI TEKPEDI	0.41	1,230.00
10		CHRISTIE NUKPEDU	0.47	1,410.00
11		DABAH MABEL	0.43	1,290.00
12		ERNEST GAKPETOR	0.23	690.00
13		REJOICE HODOGBEY	0.54	1,620.00
14		CHARITY AVORNU	0.42	1,260.00
15		TOKO AGBEKO	0.37	1,110.00
16		SETA GBETORNU	0.23	690.00
17		FRANCIS DAITEY	0.33	990.00
18		ISRAEL AMENUME	0.23	690.00
19		ALIFE EMMANUEL	0.57	1,710.00
20		TORFU BEN	0.26	780.00

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
21		FLORENCE KWAO	0.23	690.00
22		GBETORNU GLADYS	0.16	480.00
23		DUAGBOR ABLA	0.23	690.00
24		AZIETO GODWIN	0.54	1,620.00
25		KABUTEY JAMES	0.23	690.00
26		AGNES BISSI	0.69	2,070.00
27		SETH BISSI	0.23	690.00
28		ABIGAIL GBETORNU	0.23	690.00
29		GAKPEY COMFORT	0.23	690.00
30		TORVIDE MAGGIE	0.59	1,770.00
31		DABA KWASI	0.41	1,230.00
32		AYA BISSI	0.41	1,230.00
33		KROTA EGBENOR	0.41	1,230.00
34		JHS	0.16	480.00
35		NUTSI JUDITH	0.68	2,040.00
36		NUTSI WINNER	0.68	2,040.00
37		NUTSI ABAN	0.68	2,040.00
38		NUTSI MOSES	0.68	2,040.00
39		GAMOR S.K	0.68	2,040.00
40		AVLESI KWASNI	0.68	2,040.00



CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
41		GAMOR ADGO	0.34	1,020.00
42		EMMANUEL AFLO	0.93	2,790.00
43		GAMOR AYA	0.23	690.00
44		PATIENCE AHOTO	0.23	690.00
45		GLADYS AMASUNU	3.33	9,990.00
46		ERIC KUMAH	2.16	6,480.00
47		AUSTIN DABA	1.09	3,270.00
48		SELEDZI DINAH	1.52	4,560.00
49		APOTSI VIVIAN	1.07	3,210.00
50		COMFORT DZATEY	2.44	7,320.00
51		TSE NUTSI	0.38	1,140.00
52		DABA NUTSI	1.11	3,330.00
53		NOVIYA NUTIFAFA	1.13	3,390.00
54		AHENU RICHARD	1.13	3,390.00
55		SIMON AHENU	0.37	1,110.00
56		MICHAEL GBETORNU	0.37	1,110.00
57		AZIETO GODWIN	0.23	690.00
58	<b>AVEGAME</b>	ADRA COMFORT	5.56	16,680.00
59		AVEGAME COMMUNITY	17.79	53,370.00
60		KWADZO AMEKA	3.33	9,990.00

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
61		DOE GAMOR	1.11	3,330.00
62		AFI GAMOR	1.11	3,330.00
63		JULIANA BRONI	1.11	3,330.00
64		CHARITY GLAH	2.22	6,660.00
65		PRINCE ZAO	11.12	33,360.00
66		ALHAJI HUSEINI	2.22	6,660.00
67		DZIGBORDI AHOFO	0.57	1,710.00
68		CHARIMAH VIVIAN	0.57	1,710.00
69		MOSES TOVIDE	1.40	4,200.00
70		KWASI HENRY AKPALIGA	0.56	1,680.00
71	<b>ALABOTA</b>	APOTSI WISDOM	3.33	9,990.00
72		JAMES WISDOM	2.22	6,660.00
73		SELINA AKPABLI	0.74	2,220.00
74		HOPE SELIDZI	0.30	900.00
75		VICTOR DUGBATEY	0.89	2,670.00
76		SARAH DUGBATEY	1.56	4,680.00
77		MAVIS TORKU	0.44	1,320.00
78		JOHNSON GLOVER	1.60	4,800.00
79		W.K GLOVER	0.57	1,710.00
80		DOE ANABLE	0.38	1,140.00

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
81		SIMON SELEDZI	0.24	720.00
82		SENYO TSIKATA	1.06	3,180.00
83		UNKNOWN	0.33	990.00
84		FIAKEDZI JOSEPH	5.92	17,760.00
85		SIMON AVUWOADA	5.93	17,790.00
86		SIMON AKORGRE	5.94	17,820.00
87		ELLEN ADABRAH	8.45	25,350.00
88		DORA HOMADI	221.95	665,850.00
89		FRANCIS HOMADI	1.33	3,990.00
90		HELLEN ZOTOR	6.67	20,010.00
91		ALBERT DOKU	1.89	5,670.00
92		MICHAEL AGBESI	4.45	13,350.00
93		PATRICK HOMADZI	4.03	12,090.00
94		AKUA TEFEAKOR	4.03	12,090.00
95	<b>KPONKPO</b>	EMMANUEL AWUDZA	13.34	40,020.00
96		DODZI AWUDZA	13.34	40,020.00
97		BAETA AGBENYEGA	13.34	40,020.00
98		PAULINA AWUDZA	4.45	13,350.00
99		MAWENYA AWUDZA	4.45	13,350.00
100		GEOGINA SEGBEFIA	5.34	16,020.00

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
101		GEOGINA SEGBEFIA	8.00	24,000.00
102		NUKPEDU VICTOR	5.34	16,020.00
103		NUKPEDU VICTOR	8.00	24,000.00
104		ISAAC AHIALE	8.89	26,670.00
105		MOSES GADOR	4.45	13,350.00
106		PUPOLOLAMPO	1.58	4,740.00
107	<b>NORVISI</b>	S.K ELEIBLI	14.45	43,350.00
108		ENYONAM	5.56	16,680.00
109		SOLO AWULAVI	4.45	13,350.00
110		JOHNSON AGORDOKU	0.55	1,650.00
111		EKNEDZI NELSON	1.33	3,990.00
112		JULIANA AWULAVI	1.33	3,990.00
113		HUDSON DOMELI	2.23	6,690.00
114		FLORENCE DOGLI	4.45	13,350.00
115		THOMAS DOGLI	4.45	13,350.00
116		DOGLI MAMA	0.67	2,010.00
117		MARY AYIKPA	3.33	9,990.00
118		EBENEGU NUGBE	0.40	1,200.00
119		PATIENCE AKOESINI	3.03	9,090.00
120		FIAKPORNU RAPHAEL	2.80	8,400.00

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
121	OSIABORA	PATRICK ZORTORNU	2.14	6,420.00
122		GEOFEREY ZORTORNU	2.03	6,090.00
123		STEPHEN TORDZRO	3.97	11,910.00
124		BERITY DUNU	1.00	3,000.00
125		AKPANGE KWASHE	1.00	3,000.00
126		FAVOUR AGBO	0.72	2,160.00
127		STELLA AGEDE	0.28	840.00
128		JULIANA ZOTORNU	3.11	9,330.00
129		NANEDO GERSHON	2.67	8,010.00
130		SIMON ATAGBOR	2.67	8,010.00
131	ADAKOPE	CHRISTIAN AWUDI	1.11	3,330.00
132		KOBLA AVONYO	1.00	3,000.00
133		KOFI OSABUTEY	0.73	2,190.00
134		BRIGHT KABU	2.47	7,410.00
	<b>TOTAL</b>		<b>532.13</b>	<b>1,596,390.00</b>

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
1	DUFOR ADDIDOME	GHA/DO/1	DUSOR AREA COUNCIL	112,458.50
2		GHA/DO/3	GIFTY ANIKU	76,619.69
3		GHA/DO/4	ALIFE EMMANUEL	24,126.85
4		GHA/DO/5	ALIFE EMMANUEL	20,531.89
5		GHA/DO/6	GBETORNU ANANI	85,202.66
6		GHA/DO/2	MICHAEL GBETORNU	9,429.90
7	AVEGAME	GHA/AV/1	HELENE DUWOE	109,490.43
8	ADAKORPE	GHA/ADK/1	KOFI OSABUTEY	47,184.40
9	ALABOTA	GHA/AL/1	ISAAC AMABLE	18,847.87
10		GHA/AL/2	SALO KADIRI	52,258.93
11		GHA/AL/3	SELEDZI DOE	22,893.18
12	BOBRI	GHA/BO/1	GEORGE ATSU	36,110.63
13		GHA/BO/2	COMMUNITY BORE HOLE	9,869.09
14		GHA/BO/3	SAMPSON ATAKPO	13,537.42
15		GHA/BO/4	FOSTER ATAKPO	19,512.68
16		GHA/BO/6	ATSU KWADWO	28,163.83
17		GHA/BO/5	AWUSI AFEMEKU	11,358.86
18		GHA/BO/7	ANTHONY ZOR	9,573.44
19		GHA/BO/8	SIMON ATAKPO	9,431.55
20	ASIKUMA	GHA/AK/R.1	MARTIN ADU KWADWO	168,889.30

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
21		GHA/AK/L.1	.....	47,808.90
22		GHA/AK/L.2	ASIKUMA CHIPS COMPOUND	90,935.29
23		GHA/AK/L.3	RICHARD OKYERE	2,045.60
24		GHA/AK/L.4	AMANKWA CYNTHIA	104,052.30
25		GHA/AK/L.6	KENNEDY (DECEASED)	6,473.50
26		GHA/AK/L.10	JULIANA OBESE	21,222.40
27		GHA/AK/L.7	AKWASI LARBI	93,426.88
28		GHA/AK/L.5	.....	6,630.40
29		GHA/AK/R.2	DOADANU DODZI	70,983.82
30		GHA/AK/L.12	JUSTICE M. AGBENAUTSI	6,637.30
31		GHA/AK/L.13	ASANTE KWAKU JAMES	8,925.92
32		GHA/AK/L.8	CECILIA ASARE	24,931.15
33		GHA/AK/L.9	OSEI BOAFO	24,439.02
34		GHA/AK/L.11	GLADYS OBESEBAA	127,591.65
35		GHA/AK/L.14	FRANK GAWAYME	43,076.15
36		GHA/AK/L.15	DANIAL AGYA KWA	3,936.64
37		GHA/AK/L.19	NANA DEBRA NTOW II	8,365.63
38		GHA/AK/L.16	ROSE GAGPE	16,378.45
39		GHA/AK/L.17	JOYCE ADDISON	4,413.90
40		GHA/AK/L.18	ALICE OSEI TUTU	108,460.17

CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
41		GHA/AK/L.21	KWABENA ASARE	9,770.55
42		GHA/AK/L.22A	WISDOM SISTSO	183,540.46
43		GHA/AK/L.20	KOFI SERVOR	151,449.55
44		GHA/AK/L.23	.....	2,013.84
45		GHA/AK/L.24	OSAE-GYAU EBEW	33,942.40
46		GHA/AK/L.25	EBENEZER GYAU	126,646.35
47		GHA/AK/L.29	.....	7,425.05
48		GHA/AK/L.28	SAMUEL ADZOGAH	2,737.20
49		GHA/AK/L.27	KWAME ASANTE OFORI	2,687.52
50		GHA/AK/L.26	EDWARD ANSAH	3,205.70
51		GHA/AK/L.30	VIVIAN OLONGO MAMLEY	213,180.29
52		GHA/AK/L.37	SETH ACHEAMPONG	46,182.00
53		GHA/AK/L.36	EMMANUEL QUIST ARYEH	4,708.88
54		GHA/AK/L.35	OBESE EMMANUEL	47,974.83
55		GHA/AK/L.34	MARTIN DEBRAH BROWN	97,444.94
56		GHA/AK/L.32	DOTSE SUSUSZIANA	1,770.63
57		GHA/AK/L.32A	CYNTHIA NAKWA	4,636.30
58		GHA/AK/L.31	KALIFA YASER	341,215.63
59		GHA/AK/L.38	VERONICA ROSEMARY ADAMS	388,274.43
60	OSAIBRA	GHA/OS/1	VICTOR NYANSAFO	284,681.35



<b>CONSTRUCTION OF DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)</b>				
<b>INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES</b>				
<b>NO</b>	<b>TOWN</b>	<b>DEMOLISHING MARK</b>	<b>NAME</b>	<b>GHA ASSESSED VALUE (GHC)</b>
	<b>TOTAL</b>			<b>3,659,714.03</b>

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
1	DUFOR ADIDOME	PETER ZIGI	542.54
2		NURPEDU CHRISTIANA	1,569.81
3		MARY KWAO	907.72
4		WISDOM AZIATOR	2,315.04
5		KWASI DZETORME	1,041.75
6		AGNES BISI	66.22
7		AHOTOR PATIENCE	1,327.62
8		GLADYS AMANSUNU	1,432.19
9		APOTSI VIVIAN	363.53
10		MOSES TOVIDE	154.23
11	AVEGAME	ADRAH COMFORT	1,087.98
12		JULIANA OBRONI	1,022.26
13		DOE GAMO	1,604.03
14	ALABOTA	SETH SELIDZI	1,604.03
15		KOVI AFRO	513.60
16		DESMOND SELEDZI	1,159.42
17		VINCENT FIAWOYIFE	4,177.59
18		WISDOM YAYARA	2,109.38
19		ROSE ASUMA	1,438.90
20		JOHNSON GLOVER	2,017.07

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
21		DAUGBETEV VICTOR	535.13
22		REV. WILFRED KOFI GLOVER	1,594.94
23		MARY ATTISOGBE	1,771.53
24		MENSAH SEWOR	2,257.03
25		NICHOLAS GORGLAH	1,574.45
26	ADAKORPE	KOFI OSABOTEY	1,345.49
27	KOMPO	MOSES GADOH	9,966.92
28		ISAAC AHIAFRE	7,014.69
29		JOYCE DUWOE	939.71
30		VICTOR NUNEVEKPE	3,025.74
31	AVEDOTOE	NELSON PUPULAMPO	2,383.69
32		MICHAEL ABGOSI	9,410.50
33		GIDEON DUKU	12,607.23
34	DANDGE	HELENA ADDARAH	910.97
35		ATSU MAWULI EDWARD	2,071.64
36		HOMADI PATRICK	8,021.89
37		FRANCIS HOMADI	17,886.68
38		VERONICA WOEPOR	17,639.02
39		ANKU ANNA	1,407.05
40		RICHARD AWUDZA	1,626.72

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
41		SAMUEL KUGBE	1,741.62
42		KOFI ACQUAH	1,982.75
43		JULIANA OFOSUAH	2,011.13
44		VIDA AYIRKPA	3,864.35
45		HELENA ZORTOR	2,120.80
46	NORVISI	SIMON HOYA	1,603.80
47		FLORENCE DOGLI	3,184.87
48		EBENEZER NUGBE	1,029.84
49		RAPHEL FIAKPOR	10,721.61
50		PATIENCE AKWASIN	1,114.85
51		CHRISTIANA AKOTO	1,202.85
52		NORMASI KENNEDY	2,056.19
53		ROBERT AKWASIN	535.13
54		MARK KABU	1,429.25
55		FLORENCE DIKA	711.70
56		WONDER ABGETI	3,864.42
57		DOE ABGETI	400.95
58		ANAGBONU RAYMOND	801.90
59		HUDSON DOMUI	3,379.75
60		VIDA ABGETI	2,836.76

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
61		ELEBLUE MARGERET	4,940.10
62		ELEBLUE KWAME	2,122.45
63		ELEBLUE ALICE	3,110.80
64		ELEBLUE SAMUEL	31,780.72
65		ELEBLUE ENYONAM	2,004.75
66		MAWUSI AWLAVI	8,424.92
67		ALETSE GEOFREY	2,987.66
68		ALETSE MARY	2,578.07
69		ALETSE GODWIN	2,442.94
70		NELSON EKUEDZI	4,421.67
71		SOLOMON AWLAVI	2,783.92
72		JOHNSON AGORDEKU	2,405.70
73		JULIANA AWLAVI	1,202.85
74		JAMES SALOH	1,180.12
75	AMESIYAKOFI	EPHRAM ZOKLI	1,667.60
76		EDITH TSALI	1,202.85
77		NICHOLAS BOATENG	3,957.82
78	OSAIBURA	KOJO APEMEKU	3,888.50
79		NELSON HONADZI	5,862.71
80		FELICIA AFERVE	1,605.39

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
81		AKPANDOE KWASI	1,507.00
82		STEPHEN TORDZRO	3,767.50
83		MARK DORNUN	1,202.85
84		KWASI AKALIGA	753.50
85		BEAUTY DORNU	376.75
86		GEOFFERY ZORTUNU	1,507.00
87		PATRICK ZORTUNU	1,660.89
88		FAVOUR AGBO	753.50
89		GERSHON	2,857.50
90		VICTOR NYANSAFO	183,696.25
91	ASIKUMA	MARTIN WORGBEIE DELLAI	3,331.61
92		VIVIAN AWORYE	9,919.33
93		DAVID AWANGOR	1,220.61
94		YAKUBU (JACOB TSATSU)	2,004.75
95		ASIGBE GRACE	3,407.09
96		WILLIAM AFEDOE	572.79
97		MOSES HOLIDZI	2,773.27
98		FREEMAN ADZA	2,481.39
99		GALLEY BERTHA	2,447.01
100		KAFUI GAMOR	2,174.53

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
101		MARTIN DEFOR	10,648.05
102		JULIANA SAMEGE	1,507.00
103		EKLU AGBENORLAWWODZE	3,183.95
104		MARTIN ADU KWADWO	1,882.77
105		KOFI SERVOR	1,260.21
106		MARTIN DEBRAH BROWN	1,078.14
107		ASIKUMA CHIP COMPOUND	1,375.00
108		KWASI LARBI	643.37
109		SETH ACHEAMPONG	841.88
110		VERONICA ROSEMARY ADAMS	2,674.37
111	BORBRI	ATAKPO SIMON	3,458.61
112		ATAKPO FOSTER	933.90
113		GEORGE ATSU	2,091.75
114		UNKNOWN	2,308.90
115		ATAKPO DZIBERDI	463.22
116		GEORGE ATSU	4,463.72
117		KWADWO ATSU	2,771.05
118		SIMON DELLIA	1,938.47
119		ATAKPO SMPSON	5,883.36
120		ATAKPO JOHN	1,883.75

CONSTRUCTION OF DUFOR ADIDOME-ASIKUMA ROAD PROJECT (38.4 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
	TOTAL		547,318.58



CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
1	ASUTSUARE	GLADYS AKU AZAH	355.41
2		GRACE KUPONYAH	324.90
3		COMFORT DOE	151.27
4		MARY DZOTO	488.72
5		DZOTO JONAS	343.24
6		FREDRICK LEOSON	1,647.62
7		MERCY AWO	270.00
8		SAMUEL GALEAGBEME TETTEH	1,249.27
9		BOTWE ROSE	557.93
10		ABEDI P.L.	171.00
11		ERIC TETTEH	96.30
12		JOHN DZOTO	996.90
13		UNKNWON	358.83
14		DORIS ADDO	319.36
15		GRACE TAMATEY	174.33
16		KWAO AMANOR RICHARD	1,859.71
17		ROSE KORMEY	364.84
18		PETER KORNOR	4,337.59
19		SAMPSON TETTEH- EKPA	77.06
20		KWASI SOKALO	638.86

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
21		SEIDU TSAYO	3,582.32
22		EVANS ABOAGYE SEREBOUR	342.46
23		KWABENA EMMANUEL	1,226.33
24		OTIBU TETTEH ABANYO	274.50
25		TORTIMU AUGUSTINA	571.26
26		KWABENA EMMANUEL KWAO	880.56
27		ABUYAA OMEH	735.11
28		LARWEH ROBERT	370.36
29		MARGERET BODUAA	311.51
30		TORITUM AUGUSTINA	559.13
31		OPHILIA NARTEY	548.14
32		REBECCA MARTEY	585.51
33		LARMER	1,143.31
34		HALM VALATIN	461.33
35		GRACE	650.98
36		APOTA QUASHIE	1,477.50
37		FAUSTINA ZUTA	3,063.19
38	TOWN	APOSHI ESI	2,295.00
39		KORLEKWOR	577.20
40		TEYE AYEYEH	301.88

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
41		ALFRED DEDE	376.46
42		SAMUEL TETTEH	360.00
43		TEYE YAW	390.11
44		TEYE AMOS AMANOR	381.78
45		PROSPER AKOPKU	169.68
46		PETER ANYIGMOR	150.14
47		CEPHAS AMANOR	292.50
48		AKOPKU PETER	102.75
49		KARTEY DANIEL	162.89
50		MAMLEY AYERTEY COMFORT	93.15
51		LARWER COLLINS/ TEYE AYENKA HELENA	110.86
52		SIMON AGBOZO	192.75
53		AYERTEY MAMA	54.68
54		GLADYS ALI ADRA	97.61
55		PETER ANYANOR (SENIOR)	845.50
56		TEYE SIMON	180.38
57		JULIANA ANYANOR	572.61
58		REBECCA TETTEH	339.40
59		DEBRAH KANOR	1,143.00
60		AGBEZOR MERCY	268.11

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
61		PETER DEBRAH	531.97
62		LARWER CHRISTOPHER	172.16
63	ATROBBINYA	COMFORT AGBODI	266.63
64		WILLIAM AWUSI	157.50
65		LILLY AWOTEH	418.08
66		LARWER COLLINS K.	100.92
67		EDWARD MARTEY AYITEY	679.50
68		PAUL AYERTEY	352.40
69		MICHAEL KOFI AYERTEY	350.75
70		EUNICE AYERTEY	512.18
71		TIMOTHY KWAKU	90.00
72		JOYCE TEYE AYERTEY	374.50
73	AVAKPO	AKOS BODUAA	540.00
74		KWADWO KANI	1,066.68
75		TEYE SIMON	274.95
76		MABEL BOAMAH	61.65
77		GRUMAH GABRIEL	342.45
78		ELDER TEYE MAXWELL	229.82
79		KWASI AKUNA	501.38
80		UNKNOWN	151.65

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
81	VOLIVO	ALFRED TETTEH	179.81
82		GRUMAH TETTEH	154.13
83		LARWEH HONADZI	54.68
84		OBOJO HONADZI	65.61
85		RODAS NARH	226.76
86		PHILIP NARH	618.83
87		TETTEH KOFI	396.49
88		TEYE MARAGERET ADJO	163.88
89	VOLIVO DIVERSION	TETTEH HONYADZI	5,669.22
90		GLADYS EGBLE	149.72
91		BISMARCK HONYADZI	233.72
92		WAYO NARTEY	1,027.70
93		CEPHAS TEYE KWADWO	848.50
94	VOLIVO ROAD ALIGNMENT	SIMON TEYE WAYE	336.80
95		KWASI EBENEZER	365.66
96	VOLIVO BY-PASS	KWASI FIA EMMANUEL	689.33
97		PAULINA AHODE	318.53
98	VOLIVO ROAD ALIGNMENT	LINDA HOTOR	118.16
99		MERCY AHODE	82.20
100		AFIWA AMETEFEH	143.85

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
101		COMFORT TEKUTSA	159.07
102		ROSINA TORFETOR	296.47
103		WILLIAM TETTEH TOMKLU	56.51
104		COMFORT OHINPAM	250.01
105		GRUMAH KPOKPONTOH	56.51
106		MARTEY QUARSHIE	116.65
107		ELIZABETH MARKLLE	243.91
108		ISAAC NARKORTU	308.25
109	VOLIVO-LANOR TOWN	NARTEH FIO	66.79
110		FIO ADAMS	66.79
111		DIANA FIO	134.29
112		NARH GLOVER	113.74
113		CHRISTOPHA ADJOKEY	116.65
114		KANI ADJOKEY	87.48
115		EMELIA ASIMEY	94.08
116		ROSINATUFOUR	87.48
117	VOLIVO BY PASS	EMMANUEL BODUAA	67.88
118		CEPHAS TEYE KWADWO	1,272.75
119		EKLU DOE JAME KWADWO	1,173.71
120		JOSEPH ADETAH	2,139.68

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
121	CHIEF KORPE TOWN	TETTEH CLEME RICHARD	243.56
122		ERIE SEDOCAMLEA	111.52
123		ELDER JAMES	51.38
124		RAPHEL	56.51
125		TAYE ALI MODEIFA	56.51
126	ATABUI TOWN	ADZO DOGLO	49.21
127		AWO ABAYAA	118.16
128		ASIBE HAMIDU	179.81
129		KANO ERNESTINA	1,113.10
130		AFIAME WALEKU	56.51
131		STEPHEN TEYE ALI	116.65
132		SAMUEL TEYE ALI	525.04
133		JAHOE KWASHIE YEWOMOLE ASIZOBU	49.21
134		MANTSE TAWIAH GAGLOS	1,747.47
135		AWO GRUMAH	296.42
136		VERONIC PENARNH	138.78
137		YAW AGBEDEN	164.40
138		AGNES NOAGBU	61.54
139	DOFFOR TOWN	UNKNOWN	513.75
140	KORTORKOR TOWN	PHILIP ABBAN	3,317.65

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
141		REBECCA NARTYKIE	1,262.60
142		MOSES NARH ASSUMAN	390.31
143		DAVID BROWN ANYANGMOR	92.48
144		ABIGAIL DORGBADZI	22.95
145		MAJORIE	84.85
146		AGNES MENSAH	253.28
147		KWAKU FOAFO	385.31
148	TAKPONOR TOWN	DANIEL FOAFU	929.87
149		NATION WIDE CHRISTIAN FARM (DANIEL FUAFO)	763.65
150		ISSIFU NYUMUTSE	253.30
151		REJOYCE NYUMUTSE	152.51
152		AMA TEYE	46.24
153		DOPE	216.89
154		AKUVE TEYE MENSAH	940.10
155		TAWIAH KWASI	1,310.52
156		KORTEY AGBOKO	805.80
157		KOFI AGBOKO	605.40
158	ADAKORPE TOWN	THEOPHILUS TEYE ANIMLE	1,965.75
159		YAW ANIMLE	429.76
160		TEYE AGBOKO	213.94



CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
161		ADDY PRINCE	390.80
162		YAW ANIMLE	2,032.93
163		WEWE MENSAH	225.06
164		GLADYS ZORTONII	263.77
165		BENJAMIN-KANOR	298.21
166		BRIGHT GABBO	1,593.55
167		KARNOR BRIGHT	188.72
168		EUNICE WEWE	141.32
169		TEYE WEWE	71.93
170		WEWE ROSEMARY	129.79
171		RAPHAL KANOR	269.75
172		GIFTY LAMPTEY	159.26
173		CHRISTIAN KANOR	56.51
174		KOJO ISMAEL	56.51
175		VICTORIA DUDU	148.47
176	AMETAFU TOWN	ATAKPO JAMES	166.35
177		ATAKPO MENSAH	610.57
178		WISDOM AGBANYA	1,272.75
179		RICHARD ADOBLI	1,221.84
180	TOKPO NO. 1 TOWN	FRANCISCA ANIMLE	656.80

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
181		RICHARD NARH ANIMLE	500.66
182		TETTEH ANIMLE MALFA	106.05
183		JOSEPHINE ANIMLE	106.05
184	AGBETIKPO TOWN	AMA SUGLO	1,084.59
185		BOTI CHRISTIANA	179.81
186		KWAE HAVE	218.70
187	KAYA KORPE TOWN	JOYCE APPREH	948.30
188		EUNICE APPREH	135.23
189		AWUSI DAGADU	126.00
190		NATHAN DZIRAZA	237.04
191		LAWSON OLORTIMU	103.88
192		VICTORIA APPREH	216.76
193		NURDEEN OSUMANU	71.93
194		OSUMAN AWUDU	546.75
195		MENSAH TE DZI	149.78
196		UNKOWN	404.12
197		BABY	169.49
198		GRACE DZOGBOR DOR	681.05
199		YAW AMEDAMEDZI	60.14
200		FINA	32.81

CONSTRUCTION OF ASUTSUARE- AVEYIME ROAD PROJECT (24.0 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO	TOWN	NAME	VALUE OF CROPS (GH¢)
201		EMELIA AYBOYO	218.86
202		NORBODZI EYEI	54.68
203	AVEYIME TOWN	KOFI GAZEY	693.17
204		AWULA AGBANYO	71.93
205		UNKOWN (MARTHAS FATHERS)	46.24
206		WALKO NATSI	164.21
207		AMETA AGBAKLU	157.30
208		AGBOKO	242.07
209		FOSTUS DINKU	51.38
210		ELIZABETH GADOR	234.90
211		D.D. AWOGZA	125.75
212		CHRISTY TORKU	116.66
213		ROSE AVE MENA	118.16
214		W.O ADJEI	61.65
215		KWASI AGBLE	137.48
			<b>105,573.63</b>

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
1	AVEYIME DIVERSION	DAN NORGBEDI	3.7	9,250.00
2		CECILIA ADETI	0.88	2,200.00
3		OLYMPIO AKPALO	0.24	600.00
4		DIMKU FAMILY	4.03	10,075.00
5		OSUMAN GIBILBRA	0.22	550.00
6		AGNES NORGBEDI	0.31	775.00
7		GLADYS NORGBEDI	0.31	775.00
8		FRANCIS	1.02	2,550.00
9		TORGBE TORKLE	2.48	6,200.00
10		J.K. TREBAH	0.89	2,225.00
11		PAULIA NYAYYA	0.12	300.00
12		TORGBE TORKLE	3	7,500.00
13		GLADYS DINKA	0.2	500.00
14		LYDIA AGBAKLA	0.56	1,400.00
15		SIMON AGBAKLA	1.79	4,475.00
16		AMEVOR KORSHIE	1.33	3,325.00
17		AMETEPE AGBAKLA	0.67	1,675.00
18		PAPA TORGBEN	0.07	175.00
19		AWUDZA	0.22	550.00
20		DADA TSAKPA	0.22	550.00

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BARE LANDS				
NO	TOWN	NAME	FARM/LAND SIZE (ACREAGE)	LAND VALUE (GH¢)
21		AMETEPE AGBAKLA	0.37	925.00
22		TORGBE KUMI	2.22	5,550.00
23		GEDEMA KUMA	0.33	825.00
24		EMMA AMEDZA	0.3	750.00
25		YAW AWUDZA	0.3	750.00
26		KWAKU TREBAH	0.61	1,525.00
27		MARTHA M. LARWEH	0.17	425.00
28		KWASHIE MARTEY	1.24	3,100.00
29		JOYCE TEYE	1.25	3,125.00
30		PETER T. KWAO	1.68	4,200.00
31		RAPHAEL TETTEH	0.5	1,250.00
32		AMNOR ODONKOR	8.07	20,175.00
33		ISAAC K. AMANOR	1.26	3,150.00
			<b>40.56</b>	<b>92,150.00</b>

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
1	ASUTSUARE JUNCTION	GHA/AJ/L.1	KETADODO SAVIOUR	6,618.25
2		GHA/AJ/L.2	PROSPER DOGBATEY	25,784.44
3		GHA/AJ/L.3	ISAAC AMUDJO	4,265.24
4		GHA/AJ/L.4	AMUDJO BENJAMIN	2,385.68
5		GHA/AJ/L.5	JOSHUA AMUDJO	3,738.08
6		GHA/AJ/L.6	AMO TETTEH KORTEY	3,263.70
7		GHA/AJ/L.7	MARTIN KORANTENG	4,799.53
8		GHA/AJ/L.8	CHRISTIAN TORMEKPEY	2,821.53
9		GHA/AJ/L.9	JACOB K. ZUTA	2,958.38
10		GHA/AJ/L.10	FATI USMAN	2,652.94
11		GHA/AJ/L.11	ALIFA LAMIN JALLO	31,790.03
12		GHA/AJ/L.12	MARY ZUTA	4,674.75
13		GHA/AJ/L.13	ABASS TAHIRU	3,360.30
14		GHA/AJ/L.14	ABASS TAHIRU	4,846.56
15		GHA/AJ/L.15	SIMON TETTEH	3,133.18
16		GHA/AJ/L.16	GEORGE OWURAKU	3,478.64
17		GHA/AJ/L.17	MICHAEL ADJOKATSE	1,519.59
18		GHA/AJ/L.18	SAMPSON DUGBOR	3,494.85
19		GHA/AJ/L.19	FLORA AZU	3,567.88
20		GHA/AJ/L.20	PHILIP ADJOKATSE	504.56

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
21		GHA/AJ/L.21	CELESTINE TOGBENU	4,647.73
22		GHA/AJ/L.22	VICTORIA AMEWUYOR	4,961.68
23		GHA/AJ/L. 23	SARA FAYOSEY	77,484.13
24		GHA/AJ/L. 24	ISAAC TAWIAH	52,835.14
25		GHA/AJ/L.25	FELIX ADJOKASTE	1,184.62
26		GHA/AJ/L.26	RAMATU SAMBO	1,157.48
27		GHA/AJ/L.27	DANIEL AMANOO	447.64
28		GHA/AJ/L.28	RUKAYATU OSUMANU	3,389.86
29		GHA/AJ/L.30	MOHAMMED ABDUKAI SEIDU	56,853.82
30		GHA/AJ/L.31	.....	2,625.80
31		GHA/AJ/L.32	HAWA AMIDU	4,638.53
32		GHA/AJ/L.33	ASANA BUBE	3,056.70
33		GHA/AJ/L.34	FUSENA NUHU	2,003.88
34		GHA/AJ/R.50	WILLIAM MENSAH	21,342.85
35		GHA/AJ/R.49	FRIMPONG	74,206.63
36		GHA/AJ/R.48	VERONICA KINGSTON	64,452.90
37		GHA/AJ/R.47	ELECTRICITY OF GHANA (KROBO DISTRICT PAY POINT)	609.10
38		GHA/AJ/R.46	ZINABU HAMMAH	2,659.38
39		GHA/AJ/R.45	LARDI ABONGWEH	5,712.74
40		GHA/AJ/R.44	MAHAMAH RAMATU	2,530.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
41		GHA/AJ/R.43	ADIZA RUGAH	2,680.65
42		GHA/AJ/R.42	ALHASSAN BOKU	4,691.89
43		GHA/AJ/R.41	MORDZIFA DOE	1,086.75
44		GHA/AJ/R.40	ELIZABETH ADJOKATSE	3,669.08
45		GHA/AJ/R.38 A	MARIAMA ALLIDU	1,250.40
46		GHA/AJ/R.38	HAWA AMADU	3,723.24
47		GHA/AJ/R.37	RACHEL AMESIMEKU	4,467.41
48		GHA/AJ/R.36	RAMATU MUSA DAGOMBA	2,999.78
49		GHA/AJ/R.35	MOSES TANYEVA	7,097.23
50		GHA/AJ/R.34	COMFORT OTIKU	1,899.23
51		GHA/AJ/R.33	JOHN ADJOKATSE	6,797.08
52		GHA/AJ/R.30	UNKNOWN.	36,449.25
53		GHA/AJ/R.29	NAGADZI LUWORKO	27,194.63
54		GHA/AJ/R.28	JANET TAWIAH	21,273.85
55		GHA/AJ/R.27	JOHN TAWIAH	124,890.58
56		GHA/AJ/R.26	ADISA RUGA	103,625.35
57		GHA/AJ/R.25	ADIZA RUGA	154,931.45
58		GHA/AJ/R.17	BERNICE KPOH	23,766.82
59		GHA/AJ/R.18	JOHN KENNEDY KPOH	31,579.00
60		GHA/AJ/R.31	TEIN SAMBO	6,969.00



CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
61		GHA/AJ/R.32	.....	2,001.58
62		GHA/AJ/R.15	ENOCK ODUTEY	19,437.30
63		GHA/AJ/R.16	MATTHEY TETTEH	9,333.29
64		GHA/AJ/R.14 A	REJOICE TETTEH	4,775.38
65		GHA/AJ/R.14	ENOCK KONOTEY ADJEDU	76,205.67
66		GHA/AJ/R.14 B	RICHMOND KONOTY MANWEH	15,700.95
67		GHA/AJ/R.13	EMMANUEL BATSA OBU	930.64
68		GHA/AJ/R.12	STELLA AMEKO	3,422.98
69		GHA/AJ/R.11	KOFI ROBERT	7,115.63
70		GHA/AJ/R.10	ALEX ODONKOR	3,087.75
71		GHA/AJ/R.19	AMINA MAHAMDU	3,020.48
72		GHA/AJ/R.20	AJARA OSUMANU	96,657.50
73		GHA/AJ/R.23	MUSTAPHA MUTARI	90,706.25
74		GHA/AJ/R.24	AGO SAYTU TETTEH	22,514.70
75		GHA/AJ/R.22	ZINABU YARO	46,148.93
76		GHA/AJ/R.21	BAAH	39,826.80
77		GHA/AJ/R.8	GLADYS ATAKPA	21,045.00
78		GHA/AJ/R.9	GLADYS ATALKPA	48,818.08
79		GHA/AJ/R.6	KONYO CUDJOE RINDOPH	51,191.10
80		GHA/AJ/R.5	FELIX ADJOKATSE	33,850.02

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
81		GHA/AJ/R.4	FELIX ADJOKATSE	43,947.71
82		GHA/AJ/R.3	AKU LA	22,011.00
83		GHA/AJ/R.2	TERPEY ADJOKATSE	96,268.80
84		GHA/AJ/R.1	STEP WISE GROUP LTD (DANIEL MARTEY)	163,150.73
85		GHA/AJ/R.7	FRANCIS NARTEY	481,854.37
86		GHA/AJ/L.35	TAXI STATION (STEPHEN ATTETHEY)	2,820.62
87		GHA/AJ/L.36	CGBC GEZHOUBA GROUP (GHANA KPONG WATER SUPPLY EXPANSION PROJECT)	320,180.13
88		GHA/AJ/L.37	APOSTOLIC CHURCH	1,721.25
89		GHA/AJ/L.38	TEYE GILBERT	2,895.93
90		GHA/AJ/L.39	ATTO ADJOKATSE	11,780.03
91		GHA/AJ/L.40	SETH ADJOKATSE	3,500.72
92		GHA/AJ/L.41	SETH ADJOKATSE	3,789.25
93		GHA/AJ/L.42	KWAKU ADJOKATSE	6,037.50
94		GHA/AJ/L.43	KOFI	6,411.25
95	ASUTSUARE JUNCTION	GHA/AJ/R.51	OFFEI AMONI	27,383.80
96		GHA/AJ/R.53	MENSAH ONNUMA	136,821.83
97		GHA/AJ/R.52	ONUMA MENSAH	12,961.65
98		GHA/AJ/R.54	AWAL MOHAMMED RABUI	280,140.00
99		GHA/AJ/R.56	NENE PADDY WAYO	42,150.38
100		GHA/AJ/R.55	NENE PADDY WAYO	21,778.13

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
101		GHA/AJ/R.56 A	DENNIS NARTEY	25,946.88
102		GHA/AJ/R.57 A	OFORI AGBO	1,574.24
103		GHA/AJ/R.57 B	BECKY AMONI	1,062.26
104		GHA/AJ/R.56	AMEDUME GRAND VILLE (TUCASA COMPANY LTD.)	289,380.25
105		GHA/AJ/R.56	AMEDUME GRAND VILLE (TUCASA COMPANY LTD.)	267,972.66
106	ASUTSUARE SUN CITY	GHA/SC/1	SUN CITY	190,952.33
107		GHA/SC/2	SUN CITY	151,764.93
108		GHA/SC/3	SUN CITY	131,934.33
109		GHA/SC/4	SUN CITY	130,857.93
110		GHA/SC/5	SUN CITY	599,260.98
111		GHA/SC/6	SUN CITY	170,128.13
112	ASUTSUARE DIVERSION	GHA/AD/2	GIDEON	71,601.30
113		GHA/AD/3	TETTEH	123,282.88
114		GHA/AD/4	ROBERT ADJOKASTE	233,496.00
115		GHA/AD/1	.....	31,339.80
116	ASUTSUARE DIVERSION	GHA/AD/19	.....	73,825.40
117		GHA/AD/20	LT. COL. AARON RICHARD OSEI	189,053.10
118		GHA/AD/17	AGBODJA ATTEH NATHAN	171,708.23

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	GHA ASSESSED VALUE (GHC)
119		GHA/AD/15	ANEGYEMENE EPHRAIM	110,946.83
120		GHA/AD/16	.....	156,728.33
121		GHA/AD/14	ISHAQ KHADIJAH	40,283.93
122		GHA/AD/13	STEPHEN YAW ZORTONU	10,031.22
123		GHA/AD/12	STEPHEN AYITTEY	30,598.05
124		GHA/AD/9	NOAH AYITTEY	11,314.28
125		GHA/AD/8	.....	58,463.70
126		GHA/AD/10	.....	352,213.38
127		GHA/AD/11	SARAH DAKU	293,059.47
128		GHA/AD/8A	HENRY NTOBOU	19,760.91
129		GHA/AD/7	ERIC ASADOKOR	20,358.45
130		GHA/AD/7A	DZIFA ASADOKOR	12,246.93
131		GHA/AD/6	AWUKU ASADOKOR	20,472.30
132		GHA/AD/5	LYDIA SAKPITI	50,868.53
133	OSUWEM	GHA/OS/1	KWADWO LARWEH	87,717.70
134	ASUTSUARE YORKE	GHA/AY/1	.....	42,060.40
	TOTAL			7,533,162.19

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
1	ASUTSUARE JUNCTION	SYLVIA AWUMEY	0.12	480.00
2		STEPHEN AYITEY	2.00	8,000.00
3		VERA TETTEH	1.73	6,920.00
4		NENE PADY WAYO	0.23	920.00
5		DZIFA	4.94	19,760.00
6		ELIABETH	2.47	9,880.00
7		ODEI KWAKU	1.19	4,760.00
8		KENI ADJAKATSI	0.23	920.00
9		COMMUNITY LAND	8.90	35,600.00
10		FORSTER ATAKPA	0.12	480.00
11		BECK AMOANI	0.96	3,840.00
12		KWAKU ADJOKATSE	0.32	1,280.00
13		W.O KUMAH	0.16	640.00
14		DANIEL AJEIWOR	0.11	440.00
15		SAMUEL DORNU DUSI	4.89	19,560.00
16		LYDIA SEKPITI	5.00	20,000.00
17	M1(RICE FARMS)	ERNEST KOFI	0.51	1,530.00
18		JOHN NARH FIO	1.25	3,750.00
19		NARTEY OSEI	0.51	1,530.00
20		GIDEON TEYE	0.51	1,530.00
21		KWAKU AGYA	0.51	1,530.00
22		KWASI	0.51	1,530.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
23		KWAKU AKOTO	0.51	1,530.00
24		ANASI DJANBOR	0.51	1,530.00
25		DADUFI ABRODE	0.51	1,530.00
26		TEYE JOYCE	0.51	1,530.00
27		STEPHEN AKWETHEY	1.50	4,500.00
28		VIVIAN KOFI	0.51	1,530.00
29		JOHN TETTE WAYO	0.55	1,659.00
30	M2	MARGERET TEYE ALI	1.00	3,000.00
31		SOYO TEYE ALI	0.46	1,380.00
32		PAUL TEYE ALI	0.51	1,530.00
33		KOFI TEYE ALI	0.41	1,230.00
34		DORA KWASI	0.37	1,110.00
35		ISAAC AMPONSAH	0.51	1,530.00
36		EBENEZZER TEYE ALI	0.51	1,530.00
37		ABRAHAM TEYE ALI	0.51	1,530.00
38		REINDOLPH AMPONSAH	0.51	1,530.00
39		JOYCE DZOBE	0.75	2,250.00
40		MARY TETTEH	0.40	1,200.00
41		REGINA KORKOR TETTEH	0.40	1,200.00
42		GLADYS AGBLI	0.40	1,200.00
43		GLADYS OSEI	0.40	1,200.00
44		AMPONSAH REINDOLF	3.50	10,500.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
45		MERCY BABY	0.40	1,200.00
46		MOSES ADIDE TETTEH	1.50	4,500.00
47		RICHARD BRODUA	0.50	1,500.00
48		FRANCIS KWABLA	0.51	1,530.00
49		AMPONSAH REINDOLF	1.50	4,500.00
50		REGINA AMPONSAH	2.50	7,500.00
51		ROSINA AMANO	1.50	4,500.00
52		DIANA AYENSU	2.50	7,500.00
53		GUDEMA TETTEH	2.50	7,500.00
54		MABEL ASILEVU	1.50	4,500.00
55		DAVID AKUNNARH	2.00	6,000.00
56		MARGERET AKUNARH	0.50	1,500.00
57	K4	AWO KUNYIEVOR	1.00	3,000.00
58		EMMANUEL KWADJO TEYE WAYO	1.00	3,000.00
59		PHILIP AKU DOE	1.00	3,000.00
60		JOHN APAFU	1.00	3,000.00
61		REV. TETTEH THOMAS APOSORNU	1.00	3,000.00
62		COMFORT TETTEH	2.00	6,000.00
63		MALTIDA AFAFO	1.00	3,000.00
64		GLADYS NARKORTU	1.00	3,000.00
65		AWUSI ODOKORTU	1.00	3,000.00
66		FRANCIS HONYEDZI	1.00	3,000.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
67		PAULINA ABRODE	1.00	3,000.00
68		TETTEH GABRIEL	1.00	3,000.00
69		ISAAC KWASI HONYEDZI	2.00	6,000.00
70		AFI KWABLE	1.50	4,500.00
71		DIANA HONYEDZI MAKU	1.00	3,000.00
72	K3	DAVID ANSH KOFI	2.50	7,500.00
73		ABO KORKOR	1.25	3,750.00
74		DORICA HONYEDZI	1.25	3,750.00
75		EKUA DOE	1.25	3,750.00
76		HELENA TEYE	1.25	3,750.00
77		AWO GRACE	1.25	3,750.00
78		JAMES TETTEH	1.25	3,750.00
79		KWAME BIO	1.25	3,750.00
80		KWAW PETER	2.00	6,000.00
81		ARMAH KORNA	2.00	6,000.00
82		PETER KWAME ADOLF	2.25	6,750.00
83		KWASI AKUNA	2.25	6,750.00
84	K2	KWADWO KENI	2.00	6,000.00
85		DANIEL AMETEPEY	1.50	4,500.00
86		TEYE MOSES NARH-TEY	1.50	4,500.00
87		BABANYA PHILIDA	0.75	2,250.00
88		HENRY LARWEH	0.75	2,250.00



CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
89		GABRIEL GRUMAH	1.50	4,500.00
90		EVANS KWAME	1.25	3,750.00
91		JULIANA KWASHIE	1.50	4,500.00
92		GRUMAH OFONI	1.50	4,500.00
93		JOHNSON GRUMAH	1.25	3,750.00
94		TEYE MOSES	1.25	3,750.00
95		ALEX LARWEH	1.00	3,000.00
96		JAMES KORWUVI	1.00	3,000.00
97		JOSEPH BODUAH	1.00	3,000.00
98		KOTOTA AKWAVI	1.25	3,750.00
99		CYNTHIA ATALI	1.00	3,000.00
100		ABO TETTEH	1.00	3,000.00
101		MAXWELL KWASI	1.25	3,750.00
102		HARIISON APAFO	1.25	3,750.00
103	K1	BEATRICE TETTEH	2.25	6,750.00
104		DOE COMFORT	2.25	6,750.00
105		NARTEY OKOYE	1.25	3,750.00
106		THOMAS BODUAH	1.25	3,750.00
107		JULIET KWASI	1.25	3,750.00
108		SIMON KWAW FIO	1.25	3,750.00
109		EVANS ABRORNYI	1.50	4,500.00
110		MARY KWASHIE	1.25	3,750.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
111		CLEMEN ERNEST	1.25	3,750.00
112		FREDRICK DZOGBE	1.25	3,750.00
113		JOYCE ABRODE	1.25	3,750.00
114		DEBORA TETTEH	1.50	4,500.00
115		MARY TETTEH	1.00	3,000.00
116		AKOS BODUAH	0.75	2,250.00
117		NARTEY ATTAYO	0.75	2,250.00
118	J4	JOYCE TEKIE	2.50	7,500.00
119		HAPPY AKOPWOSU	0.50	1,500.00
120		JOYCE KWADJO	0.50	1,500.00
121		LARWEH HONYEDZI	1.25	3,750.00
122		MARTHA LARWEH MAMILEY	1.50	4,500.00
123		VICTORIAL SWANZI	1.25	3,750.00
124		GEORGINA ANTHONY	1.25	3,750.00
125		DANIEL ABOKA	1.25	3,750.00
126		REGINA LARWEH	1.25	3,750.00
127		SERA ABORNYI	1.25	3,750.00
128		PATRICK AYERTEY SEIDU	1.25	3,750.00
129		SAMUEL TETTEH OKOI	1.25	3,750.00
130		OLIVA KOFI	1.25	3,750.00
131		ASHADGO KWAME JOHN	1.25	3,750.00
132		VICTORIAL DZINA	1.25	3,750.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
133		GIDEON TEYE	1.00	3,000.00
134		ERIC AYERTEY	1.25	3,750.00
135		EMMANUEL AYERTEY	1.25	3,750.00
136	J3	KOFI AYERTEY	1.00	3,000.00
137		COLLINS ABORDIN	1.00	3,000.00
138		PATRICK AYERTEY SEIDU	1.00	3,000.00
139		TSETSEDEUEY DEDE	0.75	2,250.00
140		VICTORIA LARWEH	0.75	2,250.00
141		LARWEH DESMOND TEYE	1.00	3,000.00
142		COLLINS ABORDIN	1.00	3,000.00
143		AGNES ABORDIN	1.00	3,000.00
144		NACY TETTEH FIO	1.25	3,750.00
145		ANJUFOR SOME V	2.50	7,500.00
146		CHARLES KWABENA AYERTEY	2.70	8,100.00
147		GLADYS ZAINDO	2.70	8,100.00
148		BERNARD AYERTEY	1.30	3,900.00
149		EDWARD MARTEY AYERTEY	6.70	20,100.00
150	J1	LILY AWO TEYE	1.70	5,100.00
151		FRANCISA TEYE	1.70	5,100.00
152		STEPHEN TEYE KOFI	1.50	4,500.00
153		GLADYS LARWEH	1.75	5,250.00
154		HELENA TEYE AYERKOR	1.75	5,250.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
155		TIMOTHY TEYE	1.75	5,250.00
156		APOCHI SIMON	1.00	3,000.00
157		TETTEH ALFRED	1.00	3,000.00
158		COMFORT KWAME	2.00	6,000.00
159		EDWARD AYERTEY	1.25	3,750.00
160		ALFRED TETTEY	2.00	6,000.00
161		TIMOTHY TEYE	1.25	3,750.00
162		MARTEY FELIX	1.25	3,750.00
163		RICHARD AYERTEY	2.70	8,100.00
164	J2	TEYE REGINA DORCIA	1.25	3,750.00
165		AYIRTEY DORSIA	1.00	3,000.00
166		JANET AWUSI WILLIAMS	1.25	3,750.00
167		REGINA LARWEH	1.25	3,750.00
168		REBECCA TETTEH	1.25	3,750.00
169		IBRIHIM ANIYINGMOR	2.00	6,000.00
170		MARTEY PATRICK	1.50	4,500.00
171		AYERTEY EDWARD	1.50	4,500.00
172		KOKOTA ATALI	1.50	4,500.00
173		TEYE DAVID CLEVER	1.50	4,500.00
174		ROGER CHARWATEY	1.50	4,500.00
175		GLADYS ALI	1.50	4,500.00
176		EVANS KWADWO SERBUO ABOAGYE	1.00	3,000.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
177		COMFORT AGBODAM	1.00	3,000.00
178	G4	PETER DEBRAH	1.50	4,500.00
179		EMMANUEL AYERTEY	1.25	3,750.00
180		EVANS KWADWO SERBUO ABOAGYE	1.25	3,750.00
181		DIANA AYIRTEY	1.25	3,750.00
182		DZINADO CECILIA	1.25	3,750.00
183		ERIC AYERTEY	1.25	3,750.00
184		REBECCA TETTEH ABLA	1.25	3,750.00
185		DEBRA PETER	1.25	3,750.00
186		JOYCE TEYE AYERKIE	1.25	3,750.00
187		VERONIC TEYE	2.00	6,000.00
188		CHRISTIANA DEBRA	2.00	6,000.00
189		EMMANUEL AYIRTEY	1.25	3,750.00
190		JULIET AKU DOE	2.00	6,000.00
191		CHARLES KORLE LARWEH	1.25	3,750.00
192		BEATRICE TETTEH	1.25	3,750.00
193		CEPHAS AMANOR	1.25	3,750.00
194		ROGER CHARWATEY	1.25	3,750.00
195	W1	JOSEPH LARWEH KWASHIE	2.50	7,500.00
196		SAMUEL TETTEH AGOZO	0.75	2,250.00
197		KWADWO NORBEY	2.00	6,000.00
198		PHILIP AYIRTEY	3.00	9,000.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
199		SULE APOCHI	1.75	5,250.00
200		ADAMS ANIYIMOR	1.25	3,750.00
201		ANIYIMOR ROSE	1.25	3,750.00
202		ALFRED TETTEH	3.50	10,500.00
203	W2	ISAAC KWAME AMANOR	3.00	9,000.00
204		AFIMA KWABENA	1.50	4,500.00
205		MATTHEW TETTEH	1.25	3,750.00
206		ISAAC ADDY	1.00	3,000.00
207		AKOR AMANOR BERNICE	1.25	3,750.00
208		SIMON KWAME	3.00	9,000.00
209		GLADYS ABENA ALI	0.50	1,500.00
210		REBECCA TETTEH	0.35	1,050.00
211		GLADYS ALI ABENA	1.00	3,000.00
212		JANET KORLETEY	1.00	3,000.00
213		PHILIP AYERTEY	1.50	4,500.00
214	V2	AWO GEORGE	2.00	6,000.00
215		DADA	2.00	6,000.00
216		ISAAC KWAME AMANOR	2.00	6,000.00
217		THOMAS KLEME PIOGAH	2.00	6,000.00
218		KWASI BODUAH	2.00	6,000.00
219		JAMES TETTEH	4.00	12,000.00
220	V1A	AGYA EMELIA	6.00	18,000.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
221		MOSES KWADWO	2.00	6,000.00
222	NORTH LOW LEVEL	LETICIA KWASI	2.25	6,750.00
223	V1A	GABRIEL KWASI	1.50	4,500.00
224		JOE ASGOD	1.50	4,500.00
225		UNKNOWN	2.00	6,000.00
226		AFIMA KWABENA	8.00	24,000.00
227		BEATRICE TETTEH	1.00	3,000.00
228		DOMINIC TAMATEY	1.50	4,500.00
229		ANDREWS DZOTO NARTEY	0.50	1,500.00
230		FAUSTINA TETTEH	1.25	3,750.00
231		NARH ANYNGMOR	0.50	1,500.00
232		NARH ANYNGMOR	3.25	9,750.00
233		NARH ANYNGMOR	1.00	3,000.00
234		KOFI APATO	1.00	3,000.00
235		JONES TAMATEY	1.00	3,000.00
236		EMMANUEL DAMATEY DAMEGUAH	2.00	6,000.00
237		SENYO	1.25	3,750.00
238		PETER APAFO	1.25	3,750.00
239		DENNIS AHONTON	1.25	3,750.00
240		MABEL KORLETOH	1.25	3,750.00
241		REBECCA OGBUMAH	1.25	3,750.00
242	U	JONES TAMATEY	2.00	6,000.00

CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
243		AGOKO	2.00	6,000.00
244		AMAKU ABIGAL	2.50	7,500.00
245		DANIEL ASILIVI	2.50	7,500.00
246		JOSEPH AVOVU	1.75	5,250.00
247		JAMES ASIBORVO	1.75	5,250.00
248		TETTEH KONI	10.00	30,000.00
249		LARWEH	1.00	3,000.00
250		TETTEH KONI	1.00	3,000.00
251		ENOCK ABODU MENSAH	1.00	3,000.00
252		KORKOR	3.00	9,000.00
253		TETTEH BAKO	2.50	7,500.00
254		AFIYO	1.25	3,750.00
255		KWAKU EMMANUEL	1.00	3,000.00
256		JOE LEADER	2.00	6,000.00
257		SELINA	2.00	6,000.00
258		JOE AVOVO	1.50	4,500.00
259		TETTEH BOKO	1.00	3,000.00
260		AHORTO	1.00	3,000.00
261		AGBLOM	0.50	1,500.00
262		COMFORT AMBGO	1.00	3,000.00
263	V3	GLADYS TORKLO	4.00	12,000.00
264		MOSES TETTEH	1.00	3,000.00



CONSTRUCTION OF ASUTSUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
265		TEYE WAYO DAVID	1.75	5,250.00
266		NARH DANIEL	1.00	3,000.00
267		JONATHAN TEYE	1.50	4,500.00
268		MICHAEL TETTEH	0.50	1,500.00
269	V3B	SAMUEL APEDOR	5.00	15,000.00
270		FLORA APOCHI	2.50	7,500.00
271		PEACE NARTEY	1.00	3,000.00
272		TEYE KWAKU	2.00	6,000.00
273		ERIC AYERTEY	1.25	3,750.00
274		VICTORIA AMARTEY	1.25	3,750.00
275		MAMA	3.50	10,500.00
276		MICHAEL TETTEH	1.00	3,000.00
277	VOLIVO BRIDGE SITE	NICHOLAS AMANOR DARKEY	2.00	6,000.00
278		KWASI ODONKOR	2.00	6,000.00
279		ODONKOR AMANOR	3.00	9,000.00
280		NARTEY PATRICK	3.00	9,000.00
281		NICHOLAS AMANOR DARKEY	6.00	18,000.00
282		PETER TETTEH KWAW	7.00	21,000.00
283		UNKNOWN	1.00	3,000.00
284		KOFI ATEH	5.00	15,000.00
285		JOHN QUARSHIE	3.00	9,000.00
286		REGINA AYERKOR MENSAH	1.50	4,500.00

CONSTRUCTION OF ASUSTUARE JUNCTION-VOLIVO ROAD PROJECT (28.7 KM)				
INTERIM COMPENSATION ON AFFECTED LANDS				
NO	TOWN	NAME	FARM/LAND SIZE ACREAGE	LAND VALUE (GH¢)
287		ISAAC KWAME AMANOR	5.00	15,000.00
288		APPIAH NARTEY	2.00	6,000.00
289	ASUSTUARE DIVERSION	HENRY NTOBOU	2.00	6,000.00
290		ERIC ASADOKOR	1.00	3,000.00
291		AWUKU ASADOKOR	1.00	3,000.00
292		YAW ZORTORNU	1.00	3,000.00
293		STEPHEN AYITTEY	1.00	3,000.00
294		LYDIA SAKPITI	3.00	9,000.00
295	ASUSTUARE SUN CITY	SUN CITY	3.00	9,000.00
	<b>TOTAL</b>		<b>472.67</b>	<b>1,451,389.00</b>

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
	RICE FARMS		
1	M1	ERNEST KOFI	706.86
2		JOHN NARH FIO	1,732.50
3		NARTEY OSEI	706.86
4		GIDEON TEYE	706.86
5		KWAKU AGYA	706.86
6		KWASI	706.86
7		KWAKU AKOTO	706.86
8		ANASI DJANBOR	706.86
9		DADUFI ABRODE	706.86
10		TEYE JOYCE	706.86
11		STEPHEN AKWETEEY	2,697.75
12		VIVIAN KOFI	706.86
13		JOHN TETTE WAYO	770.62
14	M2	MARGERET TEYE ALI	1,914.00
15		SOYO TEYE ALI	2,114.31
16		PAUL TEYE ALI	1,319.84
17		KOFI TEYE ALI	1,001.39
18		DORA KWASI	797.45
19		ISAAC AMPONSAH	917.24

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
20		EBENEZZER TEYE ALI	917.24
21		ABRAHAM TEYE ALI	917.24
22		REINDOLPH AMPONSAH	917.24
23		JOYCE DZOBE	1,348.88
24		MARY TETTEH	719.40
25		REGINA KORKOR TETTEH	719.40
26		GLADYS AGBLI	719.40
27		GLADYS OSEI	719.40
28		AMPONSAH REINDOLF	6,294.75
29		MERCY BABY	719.40
30		MOSES ADIDE TETTEH	2,994.75
31		RICHARD BRODUA	792.83
32		FRANCIS KWABLA	917.24
33		AMPONSAH REINDOLF	2,697.75
34		REGINA AMPONSAH	4,496.25
35		ROSINA AMANO	3,118.67
36		DIANA AYENSU	4,912.05
37		GUDEMA TETTEH	4,496.25
38		MABEL ASILEVU	3,231.92
39		DAVID AKUNNARH	5,524.20

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
40		MARGERET AKUNARH	1,328.25
41	K4	AWO KUNYIEVOR	1,663.20
42		EMMANUEL KWADJO TEYE WAYO	2,001.78
43		PHILIP AKU DOE	1,499.11
44		JOHN APAFU	1,669.80
45		REV. TETTEH THOMAS APOSORNU	2,263.14
46		COMFORT TETTEH	4,296.60
47		MALTIDA AFAFO	1,877.70
48		GLADYS NARKORTU	1,904.10
49		AWUSI ODOKORTU	2,029.50
50		FRANCIS HONYEDZI	1,798.50
51		PAULINA ABRODE	1,904.10
52		TETTEH GABRIEL	2,022.90
53		ISAAC KWASI HONYEDZI	2,880.24
54		AFI KWABLE	2,079.00
55		DIANA HONYEDZI MAKU	1,386.00
56	K3	DAVID ANSH KOFI	4,496.25
57		ABO KORKOR	1,732.50
58		DORICA HONYEDZI	1,732.50
59		EKUA DOE	1,732.50

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
60		HELENA TEYE	1,732.50
61		AWO GRACE	2,479.13
62		JAMES TETTEH	2,347.13
63		KWAME BIO	2,248.13
64		KWAW PETER	2,772.00
65		ARMAH KORNA	2,772.00
66		PETER KWAME ADOLF	3,280.86
67		KWASI AKUNA	3,118.50
68	K2	KWADWO KENI	3,306.60
69		DANIEL AMETEPEY	2,697.75
70		TEYE MOSES NARH-TEY	2,418.24
71		BABANYA PHILIDA	1,039.50
72		HENRY LARWEH	1,039.50
73		GABRIEL GRUMAH	2,316.60
74		EVANS KWAME	2,611.13
75		JULIANA KWASHIE	2,978.53
76		GRUMAH OFONI	2,697.75
77		JOHNSON GRUMAH	2,471.02
78		TEYE MOSES	2,471.02
79		ALEX LARWEH	1,916.52

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
80		JAMES KORWUVI	1,386.00
81		JOSEPH BODUAH	1,386.00
82		KOTOTA AKWAVI	2,248.13
83		CYNTHIA ATALI	1,798.50
84		ABO TETTEH	1,798.50
85		MAXWELL KWASI	2,297.91
86		HARIISON APAFO	2,297.91
87	K1	BEATRICE TETTEH	3,118.50
88		DOE COMFORT	3,118.50
89		NARTEY OKOYE	2,248.13
90		THOMAS BODUAH	1,732.50
91		JULIET KWASI	1,897.50
92		SIMON KWAW FIO	2,446.13
93		EVANS ABRORNYI	2,768.84
94		MARY KWASHIE	2,248.13
95		CLEMEN ERNEST	2,570.34
96		FREDRICK DZOGBE	2,578.13
97		JOYCE ABRODE	2,328.90
98		DEBORA TETTEH	2,763.75
99		MARY TETTEH	2,049.30

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
100		AKOS BODUAH	2,470.88
101		NARTEY ATTAYO	1,348.88
102	J4	JOYCE TEKIE	3,630.00
103		HAPPY AKOPWOSU	891.00
104		JOYCE KWADJO	1,031.25
105		LARWEH HONYEDZI	2,446.13
106		MARTHA LARWEH MAMILEY	2,994.75
107		VICTORIAL SWANZI	2,644.13
108		GEORGINA ANTHONY	2,062.50
109		DANIEL ABOKA	1,897.50
110		REGINA LARWEH	1,831.50
111		SERA ABORNYI	1,864.50
112		PATRICK AYERTEY SEIDU	2,578.13
113		SAMUEL TETTEH OKOI	2,347.13
114		OLIVA KOFI	2,314.13
115		ASHADGO KWAME JOHN	1,996.50
116		VICTORIAL DZINA	2,281.13
117		GIDEON TEYE	1,824.90
118		ERIC AYERTEY	1,758.90
119		EMMANUEL AYERTEY	1,758.90



CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
120	J3	KOFI AYERTEY	1,386.00
121		COLLINS ABORDIN	1,798.50
122		PATRICK AYERTEY SEIDU	1,798.50
123		TSETSEDUEY DEDE	1,348.88
124		VICTORIA LARWEH	1,348.88
125		LARWEH DESMOND TEYE	1,386.00
126		COLLINS ABORDIN	1,798.50
127		AGNES ABORDIN	1,798.50
128		NACY TETTEH FIO	2,248.13
129		ANJUFOR SOME V	3,663.00
130		CHARLES KWABENA AYERTEY	5,383.95
131		GLADYS ZAINDO	4,987.95
132		BERNARD AYERTEY	2,810.78
133		EDWARD MARTEY AYERTEY	13,145.55
134	J1	LILY AWO TEYE	2,632.23
135		FRANCISA TEYE	2,289.03
136		STEPHEN TEYE KOFI	2,256.03
137		GLADYS LARWEH	3,863.13
138		HELENA TEYE AYERKOR	3,440.73
139		TIMOTHY TEYE	2,920.50

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
140		APOCHI SIMON	1,716.00
141		TETTEH ALFRED	2,260.50
142		COMFORT KWAME	4,005.03
143		EDWARD AYERTEY	1,758.90
144		ALFRED TETTEY	3,597.00
145		TIMOTHY TEYE	2,413.13
146		MARTEY FELIX	2,248.13
147		RICHARD AYERTEY	5,003.05
148	J2	TEYE REGINA DORCIA	1,732.50
149		AYIRTEY DORSIA	1,584.00
150		JANET AWUSI WILLIAMS	2,116.79
151		REGINA LARWEH	2,050.79
152		REBECCA TETTEH	2,082.55
153		IBRIHIM ANIYINGMOR	3,222.29
154		MARTEY PATRICK	2,697.75
155		AYERTEY EDWARD	2,343.00
156		KOKOTA ATALI	2,343.00
157		TEYE DAVID CLEVER	2,409.00
158		ROGER CHARWATEY	2,145.00
159		GLADYS ALI	2,244.00

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
160		EVANS KWADWO SERBUO ABOAGYE	2,003.10
161		COMFORT AGBODAM	1,798.50
162	G4	PETER DEBRAH	2,499.89
163		EMMANUEL AYERTEY	1,930.50
164		EVANS KWADWO SERBUO ABOAGYE	1,930.50
165		DIANA AYIRTEY	1,831.50
166		DZINADO CECILIA	1,897.50
167		ERIC AYERTEY	1,732.50
168		REBECCA TETTEH ABLA	1,930.50
169		DEBRA PETER	2,128.50
170		JOYCE TEYE AYERKIE	1,798.50
171		VERONIC TEYE	2,970.00
172		CHRISTIANA DEBRA	3,102.00
173		EMMANUEL AYIRTEY	1,732.50
174		JULIET AKU DOE	3,257.88
175		CHARLES KORLE LARWEH	1,732.50
176		BEATRICE TETTEH	1,798.50
177		CEPHAS AMANOR	2,446.13
178		ROGER CHARWATEY	2,446.13
179	W1	JOSEPH LARWEH KWASHIE	3,465.00

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
180		SAMUEL TETTEH AGOZO	1,237.50
181		KWADWO NORBEY	3,504.60
182		PHILIP AYIRTEY	6,572.91
183		SULE APOCHI	2,425.50
184		ADAMS ANIYIMOR	3,026.93
185		ANIYIMOR ROSE	2,432.77
186		ALFRED TETTEH	6,133.05
187	W2	ISAAC KWAME AMANOR	3,916.41
188		AFIMA KWABENA	1,552.65
189		MATTHEW TETTEH	1,612.17
190		ISAAC ADDY	2,248.79
191		AKOR AMANOR BERNICE	2,776.13
192		SIMON KWAME	5,388.90
193		GLADYS ABENA ALI	1,194.44
194		REBECCA TETTEH	362.29
195		GLADYS ALI ABENA	1,420.25
196		JANET KORLETEY	897.76
197		PHILIP AYERTEY	1,880.39
198	V2	AWO GEORGE	3,597.00
199		DADA	3,597.00

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
200		ISAAC KWAME AMANOR	3,636.40
201		THOMAS KLEME PIOGAH	3,636.60
202		KWASI BODUAH	3,610.20
203		JAMES TETTEH	7,141.20
204	V1A	AGYA EMELIA	11,132.78
205		MOSES KWADWO	3,597.00
206	<b>NORTH LOW LEVEL</b>	LETICIA KWASI	3,335.55
207	V1A	GABRIEL KWASI	1,552.65
208		JOE ASGOD	1,552.65
209		UNKNOWN	2,070.20
210		AFIMA KWABENA	11,088.00
211		BEATRICE TETTEH	1,386.00
212		DOMINIC TAMATEY	2,079.00
213		ANDREWS DZOTO NARTEY	693.00
214		FAUSTINA TETTEH	1,732.50
215		NARH ANYNGMOR	876.15
216		NARH ANYNGMOR	4,504.50
217		NARH ANYNGMOR	1,752.30
218		KOFI APATO	1,798.50
219		JONES TAMATEY	1,798.50

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
220		EMMANUEL DAMATEY DAMEGUAH	3,184.50
221		SENYO	2,248.13
222		PETER APAFO	2,273.02
223		DENNIS AHONTON	2,248.13
224		MABEL KORLETOH	2,413.13
225		REBECCA OGBUMAH	2,380.13
226	U	JONES TAMATEY	3,072.71
227		AGOKO	3,762.00
228		AMAKU ABIGAL	4,760.25
229		DANIEL ASILIVI	4,760.25
230		JOSEPH AVOVU	3,213.38
231		JAMES ASIBORVO	3,147.38
232		TETTEH KONI	17,523.00
233		LARWEH	1,752.30
234		TETTEH KONI	1,752.30
235		ENOCK ABODU MENSAH	1,798.50
236		KORKOR	5,395.50
237		TETTEH BAKO	4,496.25
238		AFIYO	2,190.38
239		KWAKU EMMANUEL	1,798.50

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
240		JOE LEADER	3,597.00
241		SELINA	2,772.00
242		JOE AVOVO	2,697.75
243		TETTEH BOKO	1,930.50
244		AHORTO	1,798.50
245		AGBLOM	899.25
246		COMFORT AMBGO	1,798.50
247	V3	GLADYS TORKLO	6,231.30
248		MOSES TETTEH	1,798.50
249		TEYE WAYO DAVID	3,312.38
250		NARH DANIEL	1,798.50
251		JONATHAN TEYE	2,446.79
252		MICHAEL TETTEH	615.45
253	V3B	SAMUEL APEDOR	9,355.50
254		FLORA APOCHI	2,587.75
255		PEACE NARTEY	1,798.50
256		TEYE KWAKU	3,795.00
257		ERIC AYERTEY	2,248.13
258		VICTORIA AMARTEY	1,002.38
259		MAMA	6,294.75

CONSTRUCTION OF ASUTSUARE JUNCTION - VOLIVO ROAD PROJECT (28.7 KM)			
INTERIM COMPENSATION ON AFFECTED CROPS/FARMS			
NO.	TOWN	NAME	VALUE OF CROPS
260		MICHAEL TETTEH	1,798.50
261	VOLIVO BRIDGE SITE	NICHOLAS AMANOR DARKEY	1,522.45
262		KWASI ODONKOR	1,604.10
263		ODONKOR AMANOR	8,194.36
264		NARTEY PATRICK	3,505.50
265		NICHOLAS AMANOR DARKEY	4,483.44
266		PETER TETTEH KWAW	6,815.25
267		UNKNOWN	519.50
268		KOFI ATEH	4,282.96
269		JOHN QUARSHIE	2,692.13
270		REGINA AYERKOR MENSAH	1,874.58
271		ISAAC KWAME AMANOR	3,866.23
272		APPIAH NARTEY	1,291.63
273	ASUSTUARE DIVERSION	HENRY NTOBOU	1,112.21
274		ERIC ASADOKOR	171.60
275		AWUKU ASADOKOR	829.48
276		YAW ZORTORNU	511.47
277		STEPHEN AYITTEY	546.74
278		LYDIA SAKPITI	12,934.87
	<b>TOTAL</b>		<b>718,539.98</b>



CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
1	ASUTUARE TOWN	GHA/AS/L.1	TETTEH -NOR EVANS	34,932.40
2		GHA/AS/L.2	JACOB-TETTEH KPOH	89,672.86
3		GHA/AS/L.3	AHMED KWASI AKUFFO	66,524.86
4		GHA/AS/L.4	MERCY GUAMAAH	97,108.88
5		GHA/AS/L.5	KWASI JAMES	37,085.20
6		GHA/AS/L.6	ELIZABETH MAMLEY AGBEDAM	35,424.14
7		GHA/AS/L.7	MAKU AGBEDAM	17,777.85
8		GHA/AS/L.8	K.T.K . ABBAN	10,112.18
9		GHA/AS/L.9	EMELIA TAMAKLO	4,466.60
10		GHA/AS/L.10	COMMUNITY STAND PIPE	2,588.65
11		GHA/AS/L.11	CHRISTIAN KORNOR	969.22
12		GHA/AS/L.12	AGYEMANG NATHAN	6,992.00
13		GHA/AS/L.13	.....	5,414.20
14		GHA/AS/L.14	ERIE DZAVU	24,414.50
15		GHA/AS/L.15	DARKO KUMAH	18,747.30
16		GHA/AS/L.16	ETI KWAME	672.52
17		GHA/AS/L.17	MARY AKONOR	3,326.38
18		GHA/AS/R.4	ELIZABETH SEIDU MAMLE	17,226.31
19		GHA/AS/R.3	JAMES KOFI AMPOMA	23,460.00
20		GHA/AS/R.2	JAMES KOFI AMPOMAA	46,865.03

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
21		GHA/AS/R.1	KINGDOM HALL OF JEHOVAHS WITNESSES	131,382.33
22		GHA/AS/R.1A	OKOE YAMOH	207,419.75
23		GHA/AS/R.1B	PAUL SEIDU	18,687.50
24		GHA/AS/R.1C	MARY SEIDU	14,906.30
25		GHA/AS/L.18	ROMAN CATHOLIC BASIC SCHOOL	87,821.36
26	VOLIVO	GHA/VC/L.17	EMMANUEL GAKPO	3,665.63
27		GHA/VC/L.16	STEPHEN NARKORTEY	38,971.20
28		GHA/VC/R.15	DIANA HONYEDZI MAKU	758.43
29		GHA/VC/R.14	JAMES TETTEH	17,388.00
30		GHA/VC/R.13	TETTEH PETER KWAO	32,582.38
31		GHA/VC/L.12	JONAS EGBLI	3,663.90
32		GHA/VC/L.11	TETTEH REBECCA ABELA	20,527.50
33		GHA/VC/L.10	TETTEH REBECCA ABELA	16,302.40
34		GHA/VC/L.16A	GEORGE TEI NOI	4,711.32
35		GHA/VC/L.15	.....	49,040.60
36		GHA/VC/L.14	JOYCE HONYEDZI	7,820.00
37		GHA/VC/L.13	PATIENCE KORKOR ABATSA	2,814.05
38		GHA/VC/L.12	PATIENCE KORKOR ABATSA	3,861.24
39		GHA/VC/R. 9	FRANCIS KWABENA	65,735.15
40		GHA/VC/R. 8	JOYCE HONYEDZI	3,378.13

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
41		GHA/VC/R. 7	NARH ADDO	5,549.56
42		GHA/VC/R. 6	GLADYS BODUAH	679.65
43		GHA/VC/R. 5	GAKPO MARY	3,651.71
44		GHA/VC/R.11	PROMISE DORLEGBENU	2,693.30
45		GHA/VC/R.10	TETTEH REBECCA	20,343.50
46		GHA/VC/L.9	TETTEH REBECCA	11,834.65
47		GHA/VC/L.10A	COMMUNITY WELL	2,811.75
48		GHA/VC/L. 8	APOSOTLIC CHURCH GHANA	229,177.75
49		GHA/VC/L. 7	COMFORT ALEMAH	2,949.75
50		GHA/VC/R. 4	MARY AMEPIAVOR	4,135.40
51		GHA/VC/L. 6	TETTEH YORMOR	5,910.20
52		GHA/VC/L. 5	DAVID TEYE WAYE	6,351.11
53		GHA/VC/L. 4	DAVID TEYE WAYE	27,557.91
54		GHA/VC/R. 3	SAMUEL TETTEH	5,980.00
55		GHA/VC/R. 2	COMFORT OHIPENI	57,097.50
56		GHA/VC/R. 1	KWASI JANET	59,432.69
57		GHA/VC/L. 3	JOSEPH ADATA	14,500.58
58		GHA/VC/L. 2	GRACE TETTEH MAKU	40,482.76
59		GHA/VC/L. 1	SALIFU AMADU	56,422.45
60		GHA/VC/19	JOSEPH TEYE ALI	10,528.25

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
61		GHA/VC/20	CHRISTIAN TABERCAL CHURCH	13,585.64
62	VOLIVO BY PASS	GHA/VN/2	TETTEY AKPOKA	24,104.00
63		GHA/VN/1	DANIEL AMETEPEY	26,933.00
64		GHA/VN/3	NENE PETER HONYEDZI	192,871.68
65		GHA/VN/4	WAYO NARTEH	58,461.40
66		GHA/VN/6	DANIEL AGODEN	16,928.00
67		GHA/VN/5	EKU OLEBI - DOE	50,468.90
68		GHA/VN/7	BISMARD HONYADZI	3,378.13
69	ADAKORPE	1 GHA/AD/R.	KANOR RAPHAEL	18,010.15
70		2 GHA/AD/L.	ATIKPO MENSAH	14,253.01
71		1 GHA/AD/L.	GEORGE ATTIKPO	88,433.85
72		GHA/AD/L.1A	CYNTHIA HALBADA	16,014.90
73		GHA/AD/L.1B	FINA ATIKPO	18,090.88
74		GHA/AD/1C	SETH ATTIKPO	14,199.74
75		D GHA/AD/1	ADAKORPE COMMUNITY BOREHOLE	11,233.20
76		GHA/AD/1E	CHRISTAIN KANOR	8,133.72
77	TOKPO NO.1	1 GHA/TP/R.	BEN FOFO ANIMLE	2,313.57
78		GHA/TP/R.1A	ENOCK BESSAH ANIMLE	5,195.82
79		2 GHA/TP/R.	KANIN ANIMLE	22,734.35
80	AGBETIKPO	1 GHA/AG/R.	BOTI CHRISTIANA	5,106.00

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
81		GHA/AG/R.1A	VICENT TERPKPOR	5,876.16
82		GHA/AG/R.1B	MAWULI TEFEAKOR	11,485.40
83		GHA/AG/R.2	GEORGE AMEWUGA SUGLO	30,444.82
84		GHA/AG/R.2A	GODWIN NYABLEDZI	7,293.88
85		GHA/AG/R.2B	PETER NYABLEDZI	16,481.57
86		GHA/AG/R.2C	AKORLI DUDOR	6,275.55
87		GHA/AG/R.2D	COMFORT GLAH	88,291.25
88		GHA/AG/L.1A	ALORSE MEKPORGDE SIGBE	9,655.40
89		GHA/AG/L.1B	KUGAH PHILIP	9,492.10
90		GHA/AG/L.1C	COMMUNITY STANDING PIPE	3,099.48
91		GHA/AG/L.1	ALEX AVAYIWOE	27,909.35
92		GHA/AG/L.2	NELSON ATSUDZIDZI	19,608.88
93		GHA/AG/L.3	AKAKPO ATSUDZIZI	20,087.63
94		GHA/AG/L.1F	CHRISTOPHER AKORLI	7,712.48
95		GHA/AG/L.1E	CHRISTIAN AKORLI	126,662.15
96		GHA/AG/L.1D	YAOVI GLAH	16,107.94
97		GHA/AG/L.1C	AFI TAMAKLO	9,718.08
98		GHA/AG/L.1F	PROSPER SUGLO	16,038.71
99		GHA/AG/L.1G	JOSEPH SUGLO	17,776.47
100		GHA/AG/L.1H	THOMAS SUGLO	560.63

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
101		GHA/AG/L.1I	FRANK SENYO	74,988.05
102		GHA/AG/R.2E	EBENEZZER SUGLO	2,306.33
103	KAYAKORPE	GHA/KK/R. 1	JOYCE APRAH	162,517.43
104		GHA/KK/R. 2	BERTHA KPOBI	97,465.38
105		GHA/KK/R. 3	DAVID AMEDZAKE	24,610.92
106		GHA/KK/R. 4	DIANA FUEKPE	7,376.91
107		GHA/KK/R. 5	DAGADU EWUSI	18,607.00
108		GHA/KK/R. 6	DZIGORDZI AMEDZAKE	9,091.07
109		GHA/KK/R. 7	NATHAN HYDE DZIRESE	118,689.78
110		GHA/KK/R. 8	GODWIN TOTTIME	6,123.75
111		GHA/KK/R. 9	JOHN APREH	13,517.10
112		GHA/KK/R.11	VICTORIA APREH	19,150.95
113		GHA/KK/R.11C	ENYONAM KPOBI	97,678.13
114		GHA/KK/R.11D	EMMA AMEDZAKE	114,437.65
115		GHA/KK/R.12	COURAGE APPREH	6,889.65
116		GHA/KK/R.11A	VINCENT APPREH	23,425.16
117		GHA/KK/R.13	GBORGLAH FRANCIS	7,771.13
118		GHA/KK/R.14	APPREH FRANCIS	17,664.00
119		GHA/KK/R.15	APPREH VICTORIA	9,401.25
120		GHA/KK/R.16	KINGDOM GBORGLAH	11,188.35

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
121		GHA/KK/R.17	NOAH KOVE	13,428.09
122		GHA/KK/R.18	MOSES KOVE	11,636.85
123		GHA/KK/R.19	OSUMAN AKITU	44,320.54
124		GHA/KK/R.20	OSUMAN NURNDEEN	22,729.75
125		GHA/KK/R.21	MARGERET GORNIE	7,607.25
126		GHA/KK/R.22	TOTIMU KWASI	3,032.55
127		GHA/KK/R.23	TOTIMU SAMPSON	10,558.15
128		GHA/KK/R.24	TOTIMU BEN	12,660.81
129		GHA/KK/R.25	TOTIMU AGBEGAH	98,803.40
130		GHA/KK/R.28	TOTIMU MENSAH	6,799.95
131	ADAKORPE	GHA/AD/R.2	D/A BASIC SCHOOL	20,754.74
132		GHA/AD/R.3	PATIENCE PERNARH	3,111.90
133		GHA/AD/R.4	D/A BASIC SCHOOL	231,969.38
134		GHA/AD/R.5	MENSAH WOE WOE	53,733.89
135		GHA/AD/R.6	MENSAH WOE WOE	32,150.00
136		GHA/AD/R.7	BRIGHT KABU	26,380.08
137		GHA/AD/R.8	HILDA PENNA	15,452.90
138		GHA/AD/L.2	GETHSEMANY SALVATION GOSPEL MINISTRY	25,254.00
139		GHA/AD/L.1	GLADYS ZORTOVIE	5,653.86
140		GHA/AD/L.3	BENJAMIN KABUTEY KANOR	4,033.05

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
141		GHA/AD/R.9	COMMUNITY BORE HOLE	10,643.25
142		GHA/AD/R.10	ELIZABETH TAWIAH	13,959.05
143		GHA/AD/R.11	PRINCE ADDY	16,727.33
144		GHA/AD/R.12	FRANCIS YAW ANIMLE	24,490.75
145		GHA/AD/R.13	GLADYS NYO ANIMLE	8,480.10
146		GHA/AD/R.14	TEYE ANYANMOR	8,325.54
147		GHA/AD/R.15	STELLA ANIMLE	9,289.13
148		GHA/AD/R.16	NYUMTSE ISSIFU	30,492.83
149		GHA/AD/R.17	COMFORT FOAH	8,419.96
150		GHA/AD/R.18	ASEMA KORYO	4,991.00
151		GHA/AD/R.19	REBECCA KORYA FUAFO	9,210.81
152		GHA/AD/R.20	FUAFO LOUIS	14,029.31
153		GHA/AD/R.21	FUAFO KWADWO	2,685.25
154		GHA/AD/R.22	DANIEL FUAFO	1,455.90
155		GHA/AD/R.23	AGNES MENSAH	16,623.25
156		GHA/AD/R.24	KOTORKOR COMMUNITY BORE HOLE	11,195.25
157		GHA/AD/R.25	SAMUEL AGBODZE	18,342.96
158		GHA/AD/L.27	KENNEDY AGBODZE	10,804.25
159		GHA/AD/L.28	MARJORIE ABDIN	48,115.43
160		GHA/AD/L.29	JOSEPH FIO TETTEY	19,902.36



CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
161		GHA/AD/L.30	DAIHWAN SONG (VOCATIONAL SCHOOL)	80,753.00
162		GHA/AD/L.31	AISHIKU ASIMAH	5,031.48
163		GHA/AD/L.32	PHILIP ABAYIRE	111,209.03
164		GHA/AD/L.9	EBENZZER KWABENA OTAYA	2,087.83
165		GHA/AD/R.10	APOSTOLIC CHURCH OF GHANA	26,537.40
166		GHA/AD/R.11	JUSTINA TETTEH	19,179.47
167		GHA/AD/R.12	LYDIA TETTEH	15,276.60
168		GHA/AD/R.13	AFLO ASELEVI	3,638.60
169		GHA/AD/R.14	VERONICA KANOE	10,778.95
170		GHA/AD/R.15	JONAS...	15,430.70
171		GHA/AD/R.16	THERESAH KUMAH	7,403.13
172		GHA/AD/R.17	EDWARD TAWIAH	25,649.49
173	AMETAFOR	GHA/AM/R.1A	EDWARD TAWIAH	4,236.60
174		GHA/AM/R.2	CHARLES DOKU	16,033.07
175		GHA/AM/R.3	AKU ATTIKPO	115,785.45
176		GHA/AM/R.4	FAMOUS ATSU	85,173.60
177	ASILEVI KORPE	GHA/AK/1	PETER	28,499.88
178		GHA/AK/2	PETER	5,131.30
179	TOKPO NO.2	GHA/TS/R.1	BRONI AGORYENA	12,394.93
180		GHA/TS/R.2	EBENEZER ZORTOVIE	25,819.34

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
181		GHA/TS/R.3	FAUSTINA AYEVI	28,937.22
182		GHA/TS/R.4	CHARITY	4,244.65
183		GHA/TS/R.5	PAUL DUBA	4,833.45
184		GHA/TS/R.6	BRIGHT AGBOKO	6,308.90
185		GHA/TS/R.7	FATHER AGBOKO	2,799.33
186	TOKPO NO.1	3 GHA/TO/R.	JONATHAN ANIMLI	2,975.63
187		4 GHA/TO/R.	TATAH GODFRED	17,080.26
188		5 GHA/TO/R.	PENTECOST CHURCH	7,540.32
189		6 GHA/TO/R.	EMMANUEL	5,407.30
190		GHA/TO/R.6A	TSAKU JACOB	5,934.00
191		7 GHA/TO/R.	MORTEKPO GBEDU	6,735.78
192		8 GHA/TO/R.	AMPONSAH GYEDU	15,181.10
193		9 GHA/TO/R.	VICTORIA AKWETAY	12,108.12
194		GHA/TO/10	MALSEYS GEDU	16,856.70
195		GHA/TO/11	TETTEY SEWONU	19,687.08
196		GHA/TO/12	KINGSLEY BERKOR	13,451.90
197	AVEYIME DIVERSION (SALAM)	GHA/SA/1	JOHN AZANU	17,186.06
198		GHA/SA/2	WOND-NORGBEDZI NUKUNU	9,710.60
199		GHA/SA/3	TORGBUI TORKLA IV	6,291.65
200		GHA/SA/4	ATSU KPEDZIZA	22,672.48

CONSTRUCTION OF ASUTUARE JUNCTION-AVEYIME ROAD (24.0 KM)				
INTERIM COMPENSATION FOR AFFECTED BUILDINGS/STRUCTURES				
NO	TOWN	DEMOLISHING MARK	NAME	STRUCTURE VALUE (GHC)
201	VOLIVO TOWN	GHA/VC/L.16A	DONKOR KWASI	21,912.10
202		GHA/VC/L.16	ISAAC KWAME AMANOR	8,664.10
203		GHA/VC/L.17	.....	13,068.60
204	ASILEVI	GHA/AS/3	CASSI FARMS	51,043.90
				<b>5,622,172.28</b>

EASTERN CORRIDOR PROJECT (N2)							
SUMMARY OF INTERIM VALUES FOR COMPENSATION							
NO.	NAME OF PROJECT	DESCRIPTION	NO. OF PAP S	LAND VALUE (GH¢)	BUILDINGS/STRUCTU RES (VALUE GH¢)	CROPS/FAR MS (VALUE GH¢)	COMPENSATI ON SUM(GH¢)
	ASUTSUARE JUNCTION-VOLIVO ROAD (28.7 KM)						
A		BUILDINGS/STRUCTU RES	134	1,498,471.8 8	6,034,690.32		7,533,162.19
B		CROPS/FARMS	278			718,539.98	718,539.98
C		BARE LAND (472.67 ACREAGE)	295	1,451,389.0 0			1,451,389.00
		SUB TOTAL	707	2,949,860.8 8	6,034,690.32	718,539.98	9,703,091.17
	DUFFOR ADIDOME- ASIKUMA ROAD (38.4 KM)						
A		BUILDINGS/STRUCTU RES	60	320,553.78	3,339,160.26		3,659,714.03
B		CROPS/FARMS	120			547,318.58	547,318.58
C		BARE LAND (532.13 ACREAGE)	134	1,596,390.0 0			1,596,390.00
		SUB TOTAL	314	1,916,943.7 8	3,339,160.26	547,318.58	5,803,422.61
	ASUTSUARE JUNCTION-AVEYIME ROAD (24.0 KM)						
A		BUILDINGS/STRUCTU RES	204	399,835.82	5,222,336.46		5,622,172.28
B		CROPS/FARMS	215			105,573.63	105,573.63
C		BARE LAND (40.59 ACREAGE)	35	92,150.00			92,150.00
		SUB TOTAL	454	491,985.82	5,222,336.46	105,573.63	5,819,895.91
		SUB TOTAL	1475	5,358,790.4 7	14,596,187.04	1,371,432.19	21,326,409.69

EASTERN CORRIDOR PROJECT (N2)							
SUMMARY OF INTERIM VALUES FOR COMPENSATION							
NO.	NAME OF PROJECT	DESCRIPTION	NO. OF PAPs	LAND VALUE (GH¢)	BUILDINGS/STRUCTURES (VALUE GH¢)	CROPS/FARMS (VALUE GH¢)	COMPENSATION SUM (GH¢)
	GHA OPERATIONAL EXPENSES						306,000.00
		GRAND TOTAL	1475	5,358,790.47	14,596,187.04	1,371,432.19	21,632,409.69